

# Story Avenue Bus & Safety Improvements

Bronx Community Board 9 | April 1, 2021



# Background

# Better Buses

## Better Buses Program

- Mayor's 2019 State of the City:
  - Improve bus speeds 25%
- Better Buses Action Plan
  - Released April 2019
- Bus Priority Projects
  - 22 projects implemented in 2019
  - 16.3 miles of bus lanes installed 2020
  - 2 projects completed in the Bronx in 2020:
    - Edward L Grant Hwy
    - E 149 St



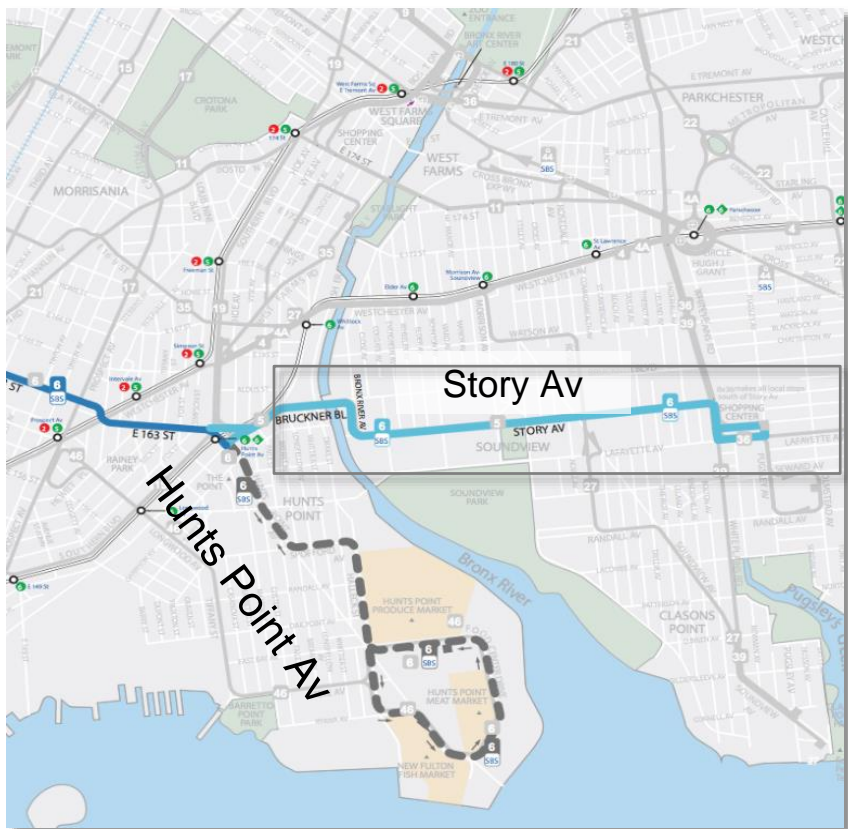
# Better Buses

## Bronx Bus Priority Corridors



- Analyzed major Bronx corridors to identify locations where treatments to improve bus speeds and reliability are needed
- Ranked potential bus-priority corridors using the following criteria:
  - Demand for bus service
  - Bus performance (speed and reliability)
  - Service levels proposed by MTA
  - Neighborhood demographics
  - Feasibility of implementation
- Selected 10 of the highest ranking corridors and have begun studying bus priority projects in the Bronx. NYC DOT work began in 2020.

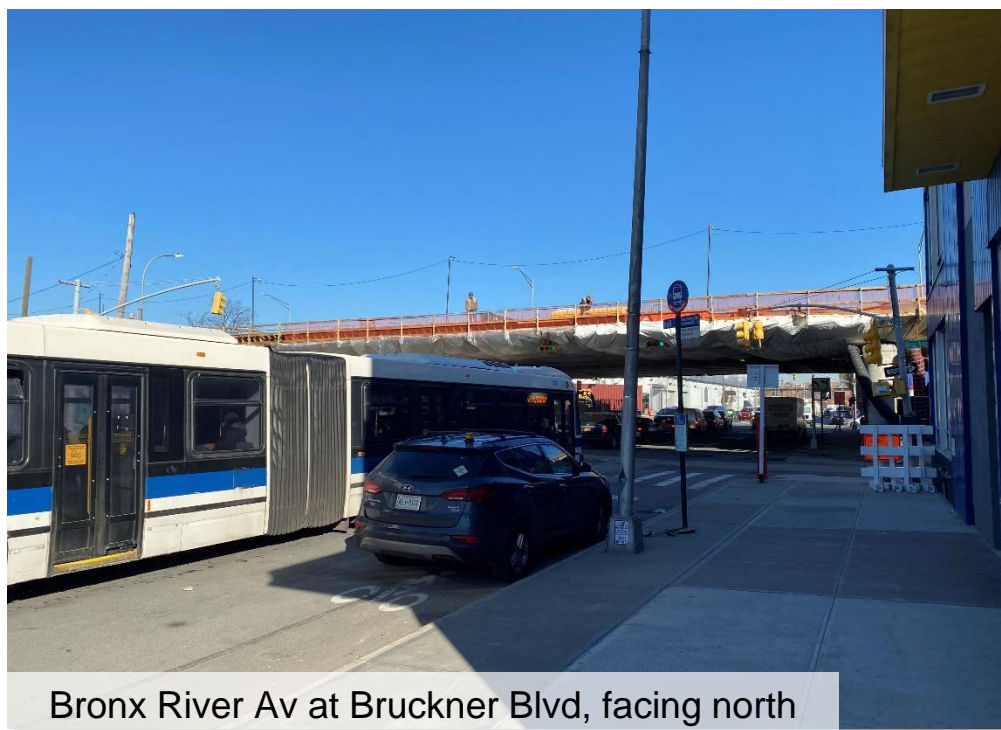
# MTA Bronx Bus Network Redesign: Story Av



- MTA Bronx Bus Network Redesign currently on hold
- MTA Bronx Bus Network Redesign proposed to:
  - Re-route Bx6 SBS east along Story Av
  - Create new Bx6 SBS terminal at Turnbull Av and Pugsley Av
  - Serve Hunts Point by Bx6 Local
  - Bx5 would continue to run on Story Av
- Proposed 2021 project will continue with some of the bus priority changes ahead of MTA's Bronx Bus Network Redesign and service changes
- Proposed 2021 project will improve existing critical bus service on Bx5, enhance east-west connections, and support future changes that would increase bus frequency on the corridor

# Reconstruction of Bruckner Expressway

- There is currently a NYS DOT project reconstructing more than a half-mile section of the Bruckner Expressway (Interstate 278)
- Construction at Bronx River Av and Bruckner Blvd may impact timeline of any changes at the intersection
- DOT will continue to monitor the area and will coordinate with State DOT and the MTA



Bronx River Av at Bruckner Blvd, facing north

# Existing Conditions

# Story Av

## Overview

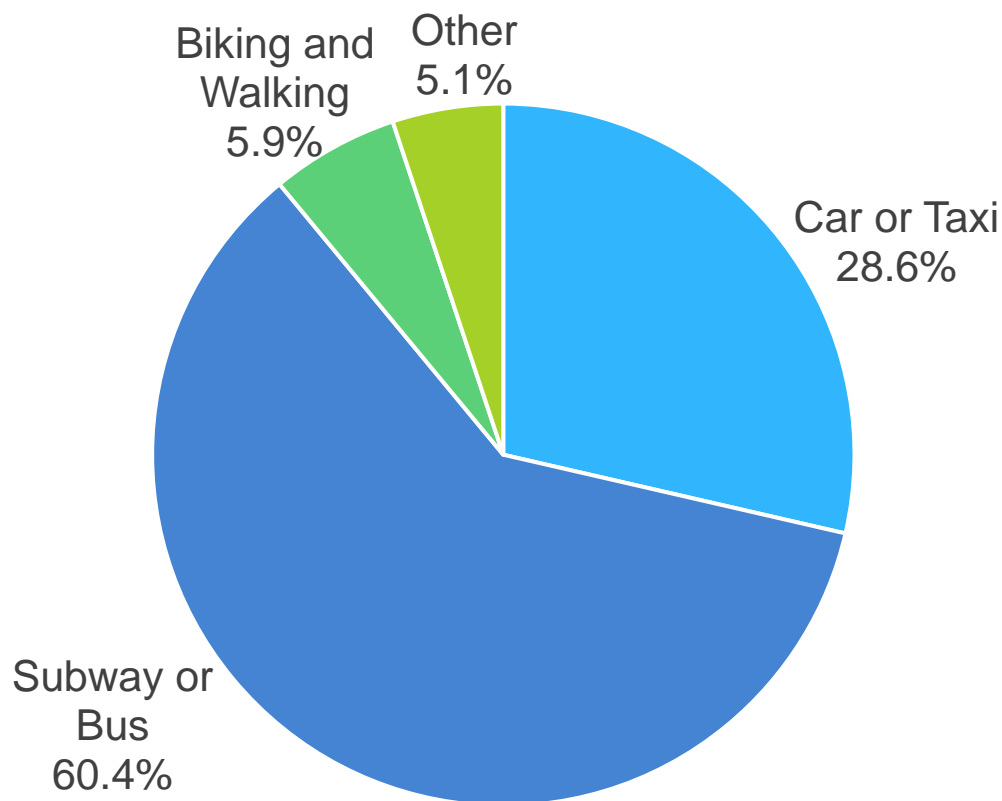
- Routes: Bx5
  - Average weekday ridership:
    - Bx5: 14,781 daily riders (May 2019)
  - Key connections to:
    - Bx6 SBS/Bx6, Bx19, Bx35 and several others
- 2 5 6**
- Average bus speeds:
    - 5-6 mph during peak periods
  - Key issues: Slow bus speeds, traffic congestion, double parking/loading, long distances between pedestrian crossing



# Community Overview

## Bronx Community Board 9

Primary Modes of Transportation to Work in CB9



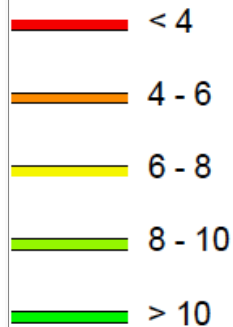
- Majority of residents living in Bronx CB 9 use public transportation to get to work
  - 60.4% use public transportation
  - 28.6% use a car/carpool
- Despite high ridership, minimal space dedicated to buses, resulting in slow bus speeds
- Bus and street improvements to the Story Av corridor would directly benefit CB 9 residents

Data source: US Census Bureau, American Community Survey 2019

# Existing Conditions

- Bus speeds are slowest during peak periods
- Speeds drop as low as 5 mph

## Bus Speeds (mph)

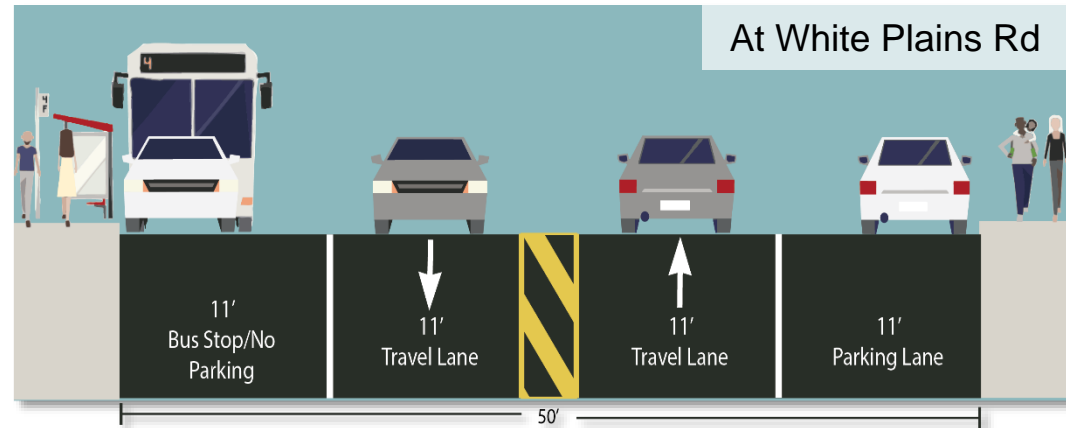


Data source: NYCT Bus Time Data, Bx5 Local Speed Data, Oct 2019

# Existing Conditions on Story Av

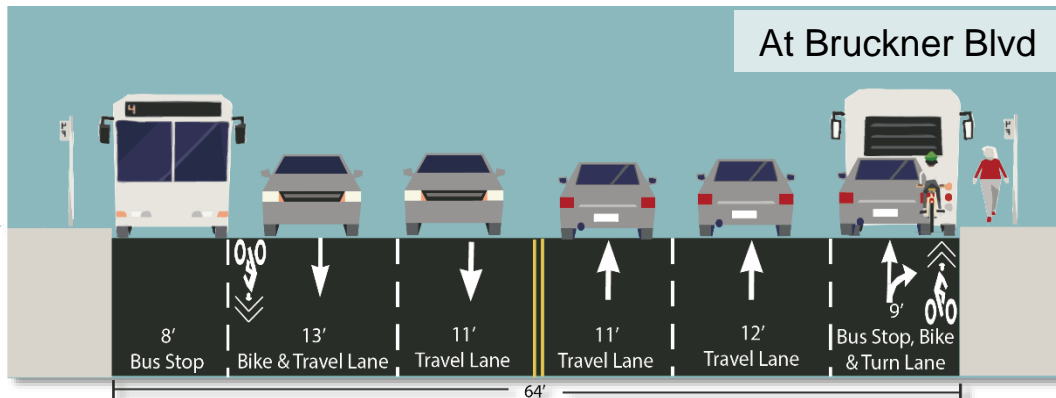


Story Av at Evergreen Av



- Key Issues:
  - Peak hour bus speeds drop to 5 mph
  - Double parking, weaving & merging
  - Vehicles blocking bus stops
  - Traffic congestion

# Existing Conditions on Bronx River Av

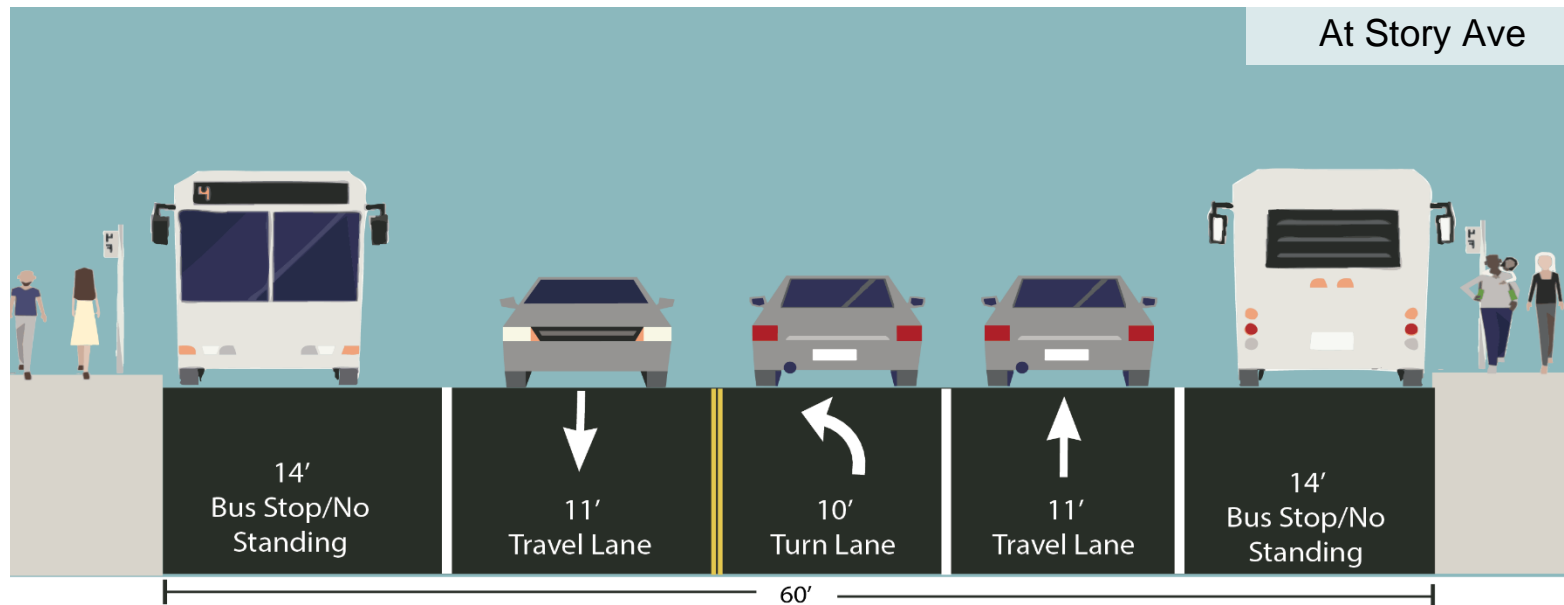


- Key Issues:
  - Peak hour bus speeds drop to 5 mph
  - Double parking, weaving & merging
  - Vehicle blocking bus stops
  - Lanes often blocked, heavy truck and auto activity
  - Traffic congestion



Bronx River Av at Bruckner Blvd

# Existing Conditions on White Plains Rd



- Key Issues:
  - AM Peak hour bus speeds drop below 5 mph
  - Vehicles blocking bus stops
  - Parking and double parking in No Standing areas of curb in front of shopping plazas

# Traffic Safety

## Story Av, Bronx River Av to White Plains Rd, BX Injury Summary, 2014-2018 (5 years)

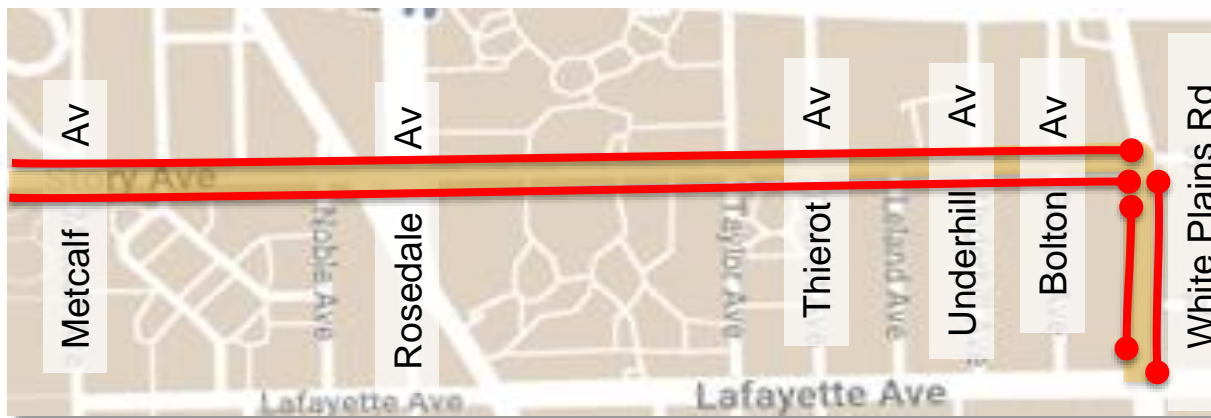
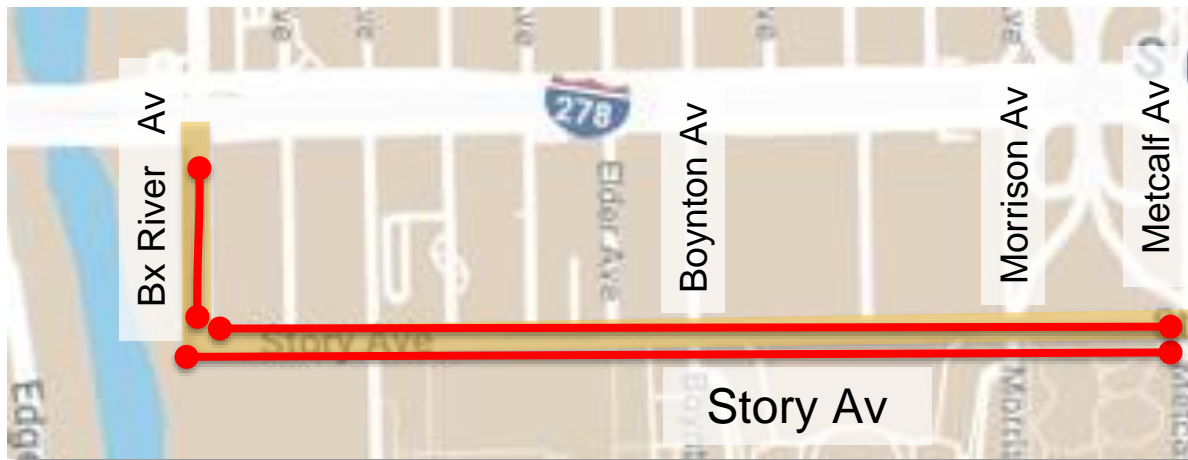
- 16 people killed or severely injured on Story Av (2014 – 2018)
- Story Av intersects with Vision Zero Priority Corridors and Intersections
- In addition to transit improvements, DOT will continue to study safety enhancements for all roadway users


|                        | Total Injuries | Severe Injuries | Fatalities | KSI |
|------------------------|----------------|-----------------|------------|-----|
| Pedestrian             | 64             | 2               | 3          | 5   |
| Bicyclists             | 16             | 2               | 0          | 2   |
| Motor Vehicle Occupant | 333            | 9               | 0          | 9   |
| Total                  | 413            | 13              | 3          | 16  |

# Proposals

# Proposed Bus Lanes

- Bus lanes on Story Avenue and White Plains Rd would maintain overnight parking throughout the corridor
- Bus lane on Bronx River Avenue would be in effect at all times



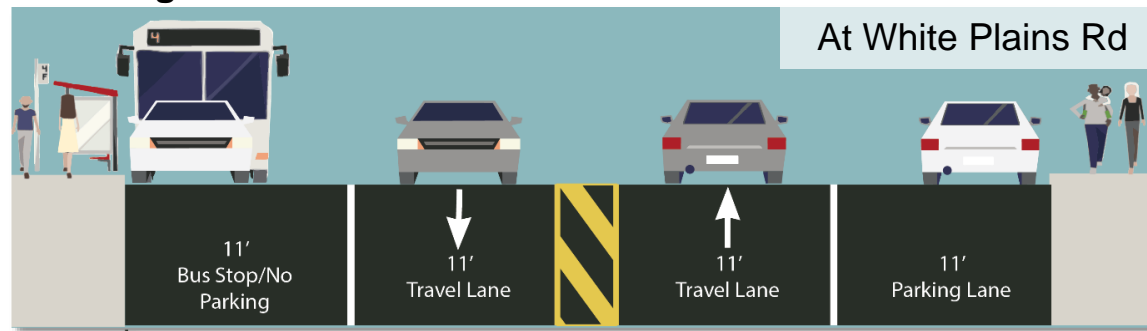
 Proposed offset or curbside bus lane

# Proposed Treatments: Story Av

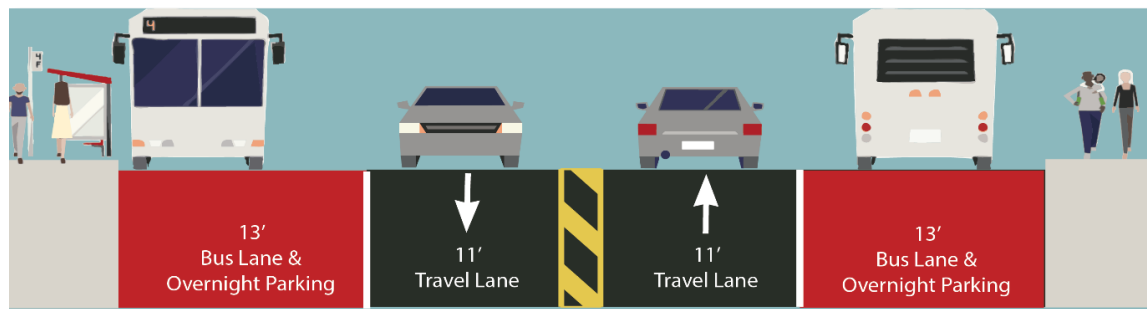
## 1. Curbside Bus Lanes on Story Av between Bronx River Av and White Plains Rd

- Issues identified:
  - Slow and unreliable bus service along congested corridor
- Curbside Bus Lane Benefits:
  - Provides dedicated road space for buses, increasing speed and reliability
  - Removes buses from general travel lanes, reducing congestion
  - Provides clearance for bus stops
  - Maintains overnight parking

### Existing

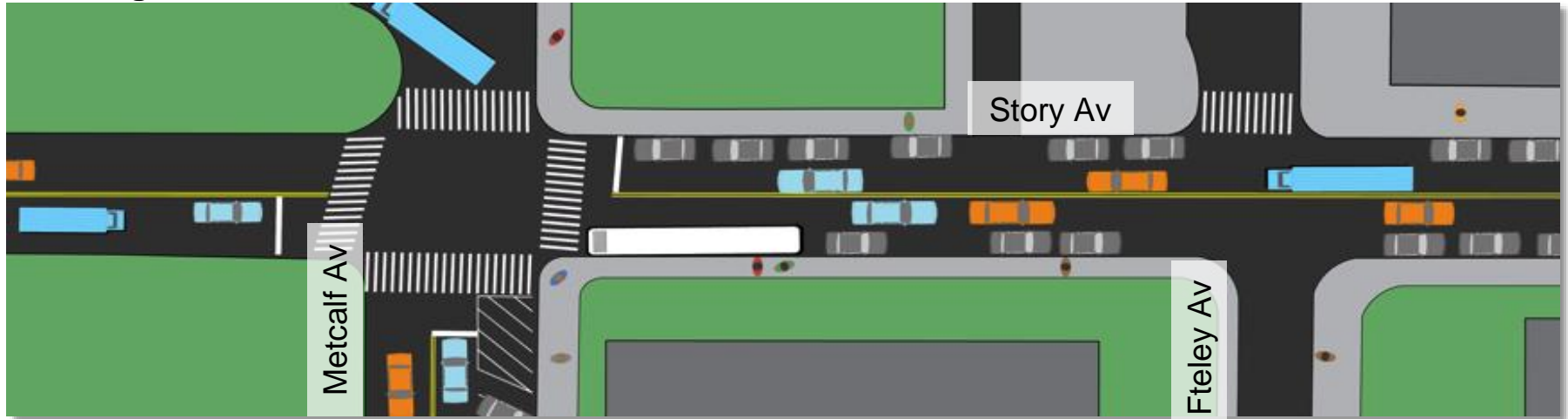


### Proposed

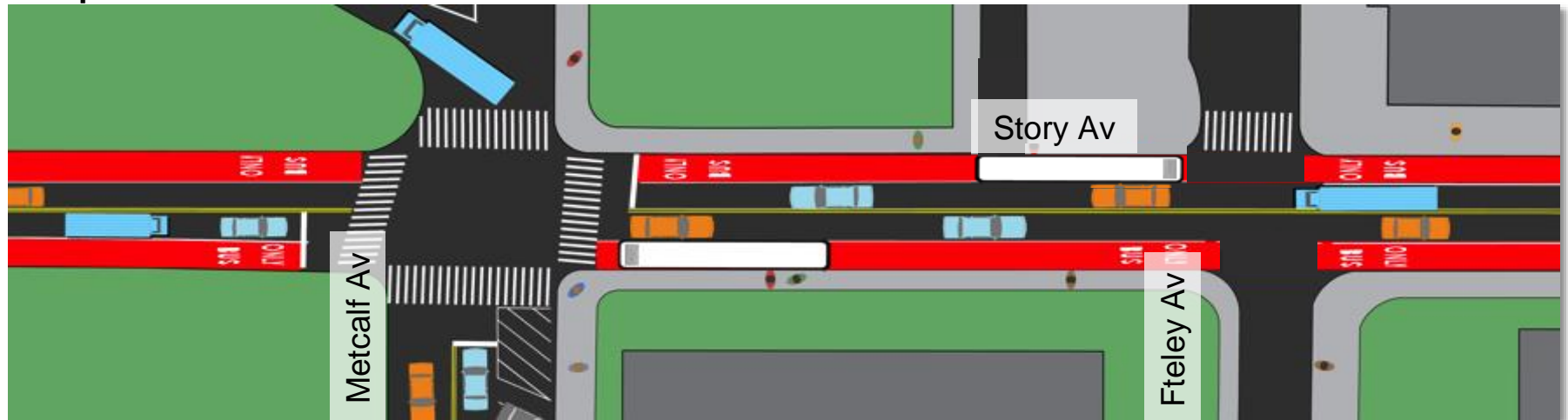


# Proposed: Story Av

Existing



Proposed

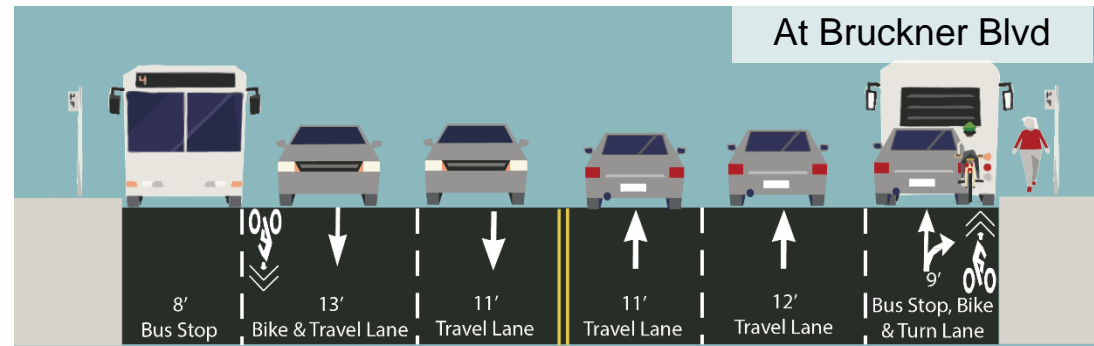


# Proposed Treatments: Bronx River Av

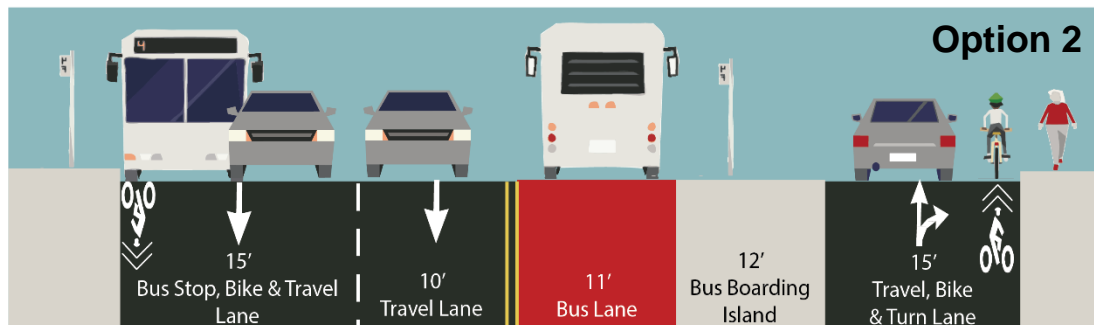
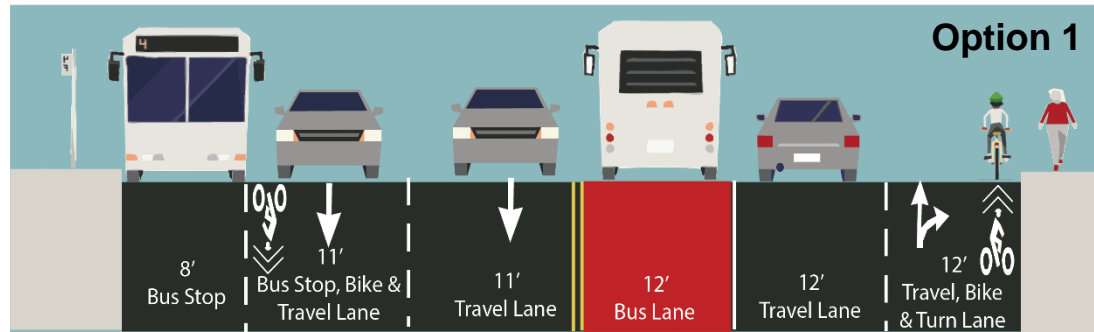
## 2. Northbound Left Side Bus Lane – Being Studied

- Issues identified:
  - Slow and unreliable bus service along congested corridor
- Left Side Bus Lane Benefits:
  - Provides dedicated road space for buses, increasing speed and reliability
  - Aligns bus with left turn at Bruckner Blvd
  - Maintains parking spaces and allows for access for industrial activities

Existing



Proposed



# Proposed Treatments:

## Story Av between Fteley Av and Croes Av

### 3. Updated Curb Regulations on Story Av between Fteley Av and Croes Av



Story Av at Fteley Av

- Issues identified:
  - Due to overflow parking for NYPD vehicles and no parking at FDNY fire zone, vehicles currently block bus stops or limit bus access to curb
  - Creates traffic, especially for buses and cars travelling eastbound and westbound in peak AM/PM hours
- Updating Curb Regulations Benefits:
  - Improves demand management, formalizes parking for authorized vehicles in designated parking areas
  - Facilitates movement of all other vehicular traffic traveling eastbound and westbound
  - Emergency vehicles can travel in bus lanes at all hours

# Proposed Treatments: Story Av at Underhill Av

## 4. Updated Curb Regulations on Story Av at Underhill Av

- Issues identified:
  - Due to Alternate Side Parking (currently 7AM to 4PM on school days), school buses must double park for student pick-up on school days during peak hours
  - Creates traffic, especially for buses and cars travelling eastbound in peak AM/PM hours
- Updating Curb Regulations Benefits:
  - Bus lane for helps with demand management, prioritizing school buses during student pick-ups and drop-offs
  - Facilitates movement of all other vehicular traffic travelling eastbound



# Proposed Treatments: Mid-block Crossings

## 5. Mid-block Pedestrian Crossing – Being Studied

- Issues identified:
  - Story Av between Rosedale Av and Taylor Av is a long block without dedicated pedestrian crossing
- Benefits of treatment:
  - Improve pedestrian safety for those accessing bus stops and crossing Story Av
- DOT is currently studying the feasibility of new, mid-block pedestrian crossing



# Proposed Treatments: Bus Stop Changes

## 6. Consolidated Bus Stops

- To improve bus speeds and reliability, seven bus stops will be consolidated on the corridor. These changes allows for additional overnight parking spaces



Bx5 Local bus stop



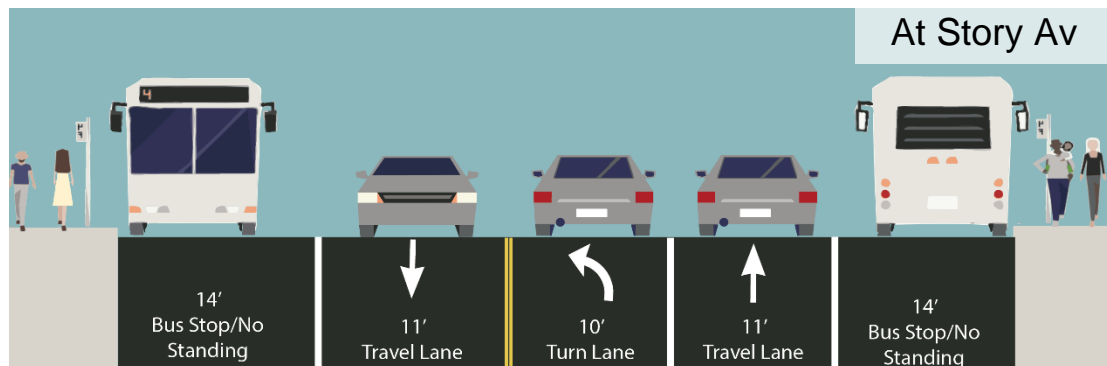
Bx5 Local bus stop to be removed

# Proposed Treatments: White Plains Rd

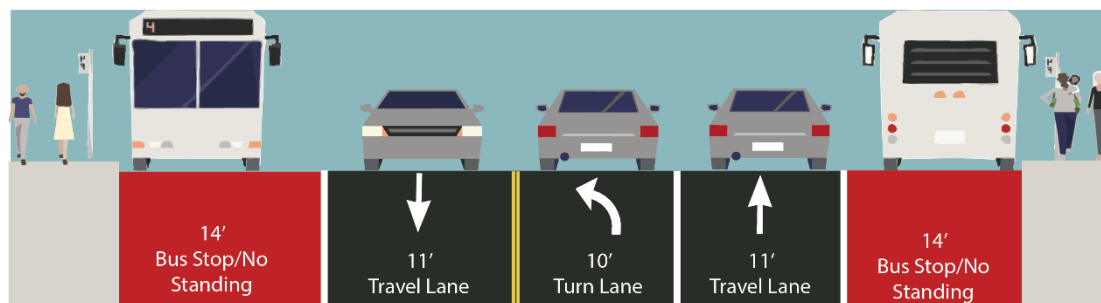
## 7. Curbside Bus Lanes on White Plains Rd between Story Av and Lafayette Av

- Issues identified:
  - Slow and unreliable bus service along congested corridor during peak hours
- Curbside Bus Lane Benefits:
  - Provides dedicated road space for buses, increasing speed and reliability
  - Removes buses from general travel lanes, reducing congestion
  - Provides clearance for bus stops

Existing



Proposed



# Parking Changes on Story Av

- Parking would be preserved overnight
- Stop consolidations would add up to 16 parking spaces overnight

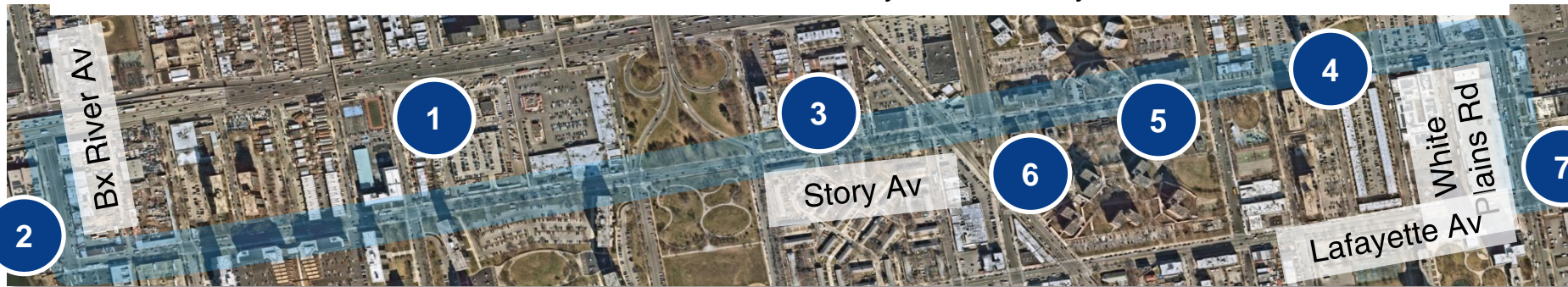
Story Av, Bronx River Av to White Plains Rd (EB & WB)

|                           | Parking   |            |
|---------------------------|---|------------|
|                           | South Curb  | North Curb |
| Current                   | 191   | 146        |
| Proposed                  | Segments of curbside bus lanes in effect during peak hours.   |            |
| Full-time Parking Changes | +11   | +5         |
|                           | Full-time parking spaces gained from bus stop removals  |            |
| Part-time Parking Changes | -172  | -130       |
|                           | Alternate side parking spaces repurposed during daytime bus lane hours; preserved for overnight parking |            |

# Proposed Treatments: Story Av

## Summary of Proposed Treatments

1. Curbside Bus Lanes on Story Av between Bronx River Av and White Plains Rd
2. Northbound Left Side Bus Lane on Bronx River Ave – *Being Studied*
3. Updated Curb Regulations on Story Av between Fteley Av and Croes Av
4. Updated Curb Regulations on Story Av at Underhill Av
5. Mid-block Pedestrian Crossing – *Being Studied*
6. Consolidated Bus Stops between Bronx River Av and White Plains Rd
7. Curbside Bus Lanes on White Plains Rd between Story Av and Lafayette Av

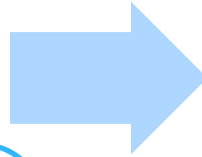


# Project Schedule

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April 2021

- Present draft plan to CB9
- Finalize design based on stakeholder feedback
- Share final plan with CB and other stakeholders



Summer 2021

- Install markings and signage
- Adjust parking regulations
- Coordinate with NYSDOT on Bruckner reconstruction

# Thank You!

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## Questions?



NYC DOT



NYC DOT



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