

ENVIRONMENTAL ASSESSMENT STATEMENT



**3939 RICHMOND AVENUE
STATEN ISLAND, NY 10312**

CEQR No. 15DCP109R

Prepared By

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ARCHITECT

4864 Arthur Kill Road, Suite 300

Staten Island, NY 10309

718.317.9600

June 12, 2015

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EAS Short Form (Part 2)

Project Summary

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- Air Quality

Appendix 'A': WRP Consistency Assessment Form



City Environmental Quality Review

ENVIRONMENTAL ASSESSMENT STATEMENT (EAS) SHORT FORM

FOR UNLISTED ACTIONS ONLY • Please fill out and submit to the appropriate agency ([see instructions](#))

Part I: GENERAL INFORMATION

1. Does the Action Exceed Any Type I Threshold in 6 NYCRR Part 617.4 or 43 RCNY §6-15(A) (Executive Order 91 of 1977, as amended)? YES NO

If "yes," STOP and complete the [FULL EAS FORM](#).

2. Project Name 3939 Richmond Avenue - YMCA Parking Lot

3. Reference Numbers

CEQR REFERENCE NUMBER (to be assigned by lead agency)
15DCP109R

BSA REFERENCE NUMBER (if applicable)

ULURP REFERENCE NUMBER (if applicable)
N150249RAR, 150406RAR

OTHER REFERENCE NUMBER(S) (if applicable)
(e.g., legislative intro, CAPA)

4a. Lead Agency Information

NAME OF LEAD AGENCY

NYC Department of City Planning

NAME OF LEAD AGENCY CONTACT PERSON

Robert Dobruskin

ADDRESS 22 Reade Street

CITY New York

STATE NY

ZIP 10007

TELEPHONE (212) 720-3423

EMAIL

rdobrus@planning.nyc.gov

4b. Applicant Information

NAME OF APPLICANT

YMCA of Greater New York

NAME OF APPLICANT'S REPRESENTATIVE OR CONTACT PERSON

Michael A. Biagioli, AIA

ADDRESS 4864 Arthur Kill Road, Suite 300

CITY Staten Island

STATE NY

ZIP 10309

TELEPHONE 718-317-
9600

EMAIL mabaia@msn.com

5. Project Description

The applicant, The YMCA of Greater New York, is seeking an authorization pursuant to Sect. ZR 107-68: Authorization of the Modification of Group Parking Facility and Access Regulations and Sect. ZR 107-65: Modification of Existing Topography. The proposed action seeks to formalize a temporary parking area. The existing project area is currently improved with 50,435 gsf of community facility uses and 107 at-grade parking spaces. The affected area is located in the Eltingville neighborhood in the Borough of Staten Island, Community District #3.

Project Location

BOROUGH Staten Island

COMMUNITY DISTRICT(S) 3

STREET ADDRESS 3939 Richmond Avenue, Staten Island,
NY 10312

TAX BLOCK(S) AND LOT(S) Block 5236, Lot 31

ZIP CODE 10312

DESCRIPTION OF PROPERTY BY BOUNDING OR CROSS STREETS The project site is located on a corner lot fronting three streets (Richmond Avenue on the south westerly side; Oakdale Street on the south easterly side; and Ridgcrest Avenue on the north easterly side of the project site)

EXISTING ZONING DISTRICT, INCLUDING SPECIAL ZONING DISTRICT DESIGNATION, IF ANY R3-2 in
the Special South Richmond Zoning District

ZONING SECTIONAL MAP NUMBER 33C

6. Required Actions or Approvals (check all that apply)

City Planning Commission: YES NO UNIFORM LAND USE REVIEW PROCEDURE (ULURP)

CITY MAP AMENDMENT

ZONING CERTIFICATION

CONCESSION

ZONING MAP AMENDMENT

ZONING AUTHORIZATION

UDAAP

ZONING TEXT AMENDMENT

ACQUISITION—REAL PROPERTY

REVOCABLE CONSENT

SITE SELECTION—PUBLIC FACILITY

DISPOSITION—REAL PROPERTY

FRANCHISE

HOUSING PLAN & PROJECT

OTHER, explain:

SPECIAL PERMIT (if appropriate, specify type: modification; renewal; other); EXPIRATION DATE:

SPECIFY AFFECTED SECTIONS OF THE ZONING RESOLUTION ZR 107-68

Board of Standards and Appeals: YES NO

VARIANCE (use)

VARIANCE (bulk)

SPECIAL PERMIT (if appropriate, specify type: modification; renewal; other); EXPIRATION DATE:
SPECIFY AFFECTED SECTIONS OF THE ZONING RESOLUTION

Department of Environmental Protection: YES NO If "yes," specify:

Other City Approvals Subject to CEQR (check all that apply)

LEGISLATION FUNDING OF CONSTRUCTION, specify:
 RULEMAKING POLICY OR PLAN, specify:
 CONSTRUCTION OF PUBLIC FACILITIES FUNDING OF PROGRAMS, specify:
 384(b)(4) APPROVAL PERMITS, specify:
 OTHER, explain:

Other City Approvals Not Subject to CEQR (check all that apply)

PERMITS FROM DOT'S OFFICE OF CONSTRUCTION MITIGATION AND COORDINATION (OCMC) LANDMARKS PRESERVATION COMMISSION APPROVAL
 OTHER, explain: Department of Buildings

State or Federal Actions/Approvals/Funding: YES NO If "yes," specify:

7. Site Description: *The directly affected area consists of the project site and the area subject to any change in regulatory controls. Except where otherwise indicated, provide the following information with regard to the directly affected area.*

Graphics: *The following graphics must be attached and each box must be checked off before the EAS is complete. Each map must clearly depict the boundaries of the directly affected area or areas and indicate a 400-foot radius drawn from the outer boundaries of the project site. Maps may not exceed 11 x 17 inches in size and, for paper filings, must be folded to 8.5 x 11 inches.*

SITE LOCATION MAP ZONING MAP SANBORN OR OTHER LAND USE MAP
 TAX MAP FOR LARGE AREAS OR MULTIPLE SITES, A GIS SHAPE FILE THAT DEFINES THE PROJECT SITE(S)
 PHOTOGRAPHS OF THE PROJECT SITE TAKEN WITHIN 6 MONTHS OF EAS SUBMISSION AND KEYED TO THE SITE LOCATION MAP

Physical Setting (both developed and undeveloped areas)

Total directly affected area (sq. ft.): 105,126 sf Waterbody area (sq. ft) and type: 0
 Roads, buildings, and other paved surfaces (sq. ft.): 50,435 gsf Other, describe (sq. ft.): 0

8. Physical Dimensions and Scale of Project (if the project affects multiple sites, provide the total development facilitated by the action)

SIZE OF PROJECT TO BE DEVELOPED (gross square feet):
 NUMBER OF BUILDINGS: GROSS FLOOR AREA OF EACH BUILDING (sq. ft.):
 HEIGHT OF EACH BUILDING (ft.): NUMBER OF STORIES OF EACH BUILDING:

Does the proposed project involve changes in zoning on one or more sites? YES NO
 If "yes," specify: The total square feet owned or controlled by the applicant:
 The total square feet not owned or controlled by the applicant:

Does the proposed project involve in-ground excavation or subsurface disturbance, including, but not limited to foundation work, pilings, utility lines, or grading? YES NO
 If "yes," indicate the estimated area and volume dimensions of subsurface permanent and temporary disturbance (if known):
 AREA OF TEMPORARY DISTURBANCE: 28,050 sq. ft. (width x length) VOLUME OF DISTURBANCE: 28,050 cubic ft. (width x length x depth)
 AREA OF PERMANENT DISTURBANCE: 28,050 sq. ft. (width x length)

Description of Proposed Uses (please complete the following information as appropriate)

	Residential	Commercial	Community Facility	Industrial/Manufacturing
Size (in gross sq. ft.)	N/A	N/A	N/A	N/A
Type (e.g., retail, office, school)	N/A units	N/A	N/A	N/A

Does the proposed project increase the population of residents and/or on-site workers? YES NO
 If "yes," please specify: NUMBER OF ADDITIONAL RESIDENTS: NUMBER OF ADDITIONAL WORKERS:
 Provide a brief explanation of how these numbers were determined:

Does the proposed project create new open space? YES NO If "yes," specify size of project-created open space: sq. ft.

Has a No-Action scenario been defined for this project that differs from the existing condition? YES NO
 If "yes," see [Chapter 2](#), "Establishing the Analysis Framework" and describe briefly:

9. Analysis Year [CEQR Technical Manual Chapter 2](#)

ANTICIPATED BUILD YEAR (date the project would be completed and operational): 2016

ANTICIPATED PERIOD OF CONSTRUCTION IN MONTHS: 4

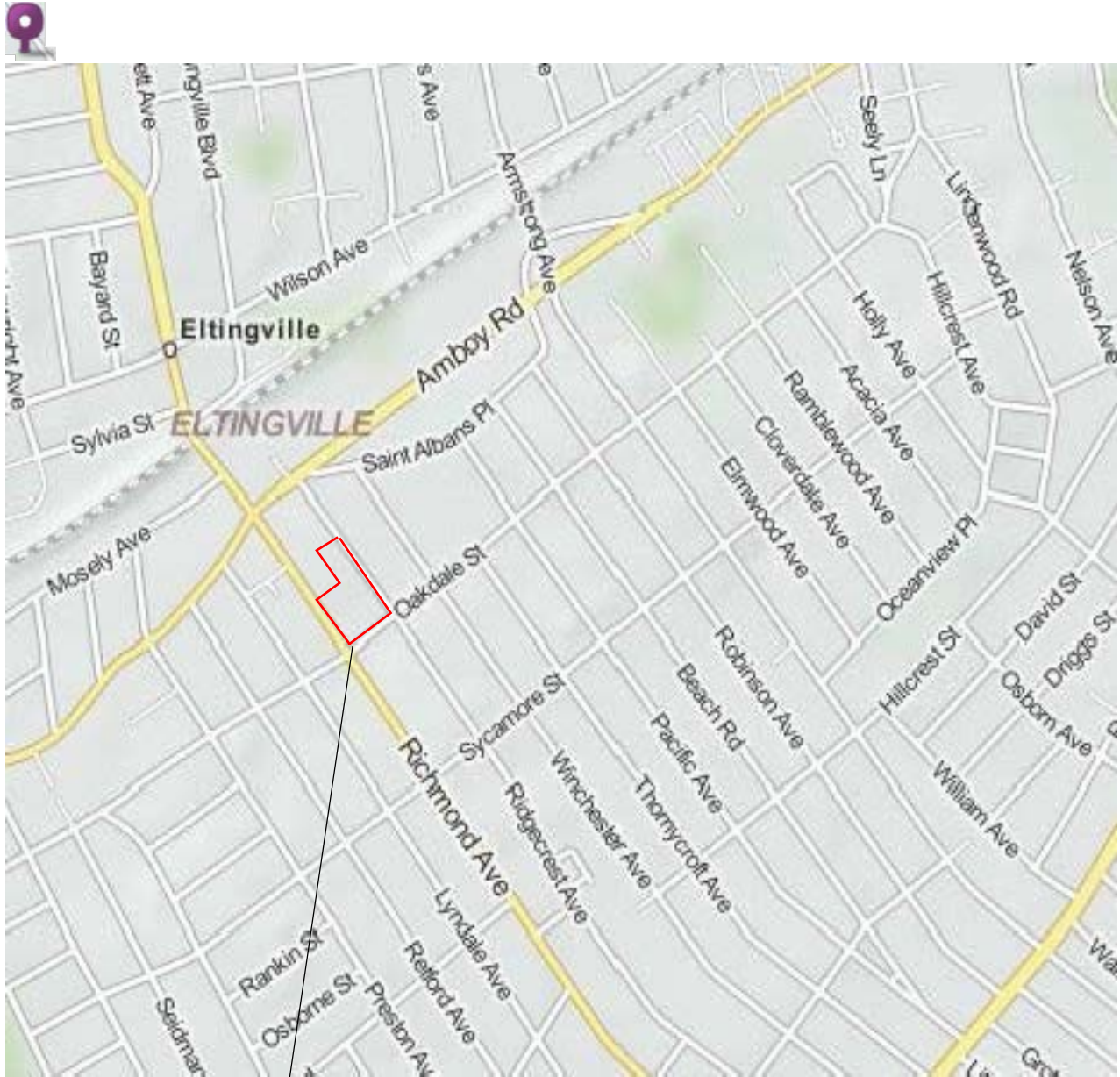
WOULD THE PROJECT BE IMPLEMENTED IN A SINGLE PHASE? YES NO IF MULTIPLE PHASES, HOW MANY?

BRIEFLY DESCRIBE PHASES AND CONSTRUCTION SCHEDULE: Site Drainage and Surface Finishing Work

10. Predominant Land Use in the Vicinity of the Project (check all that apply)

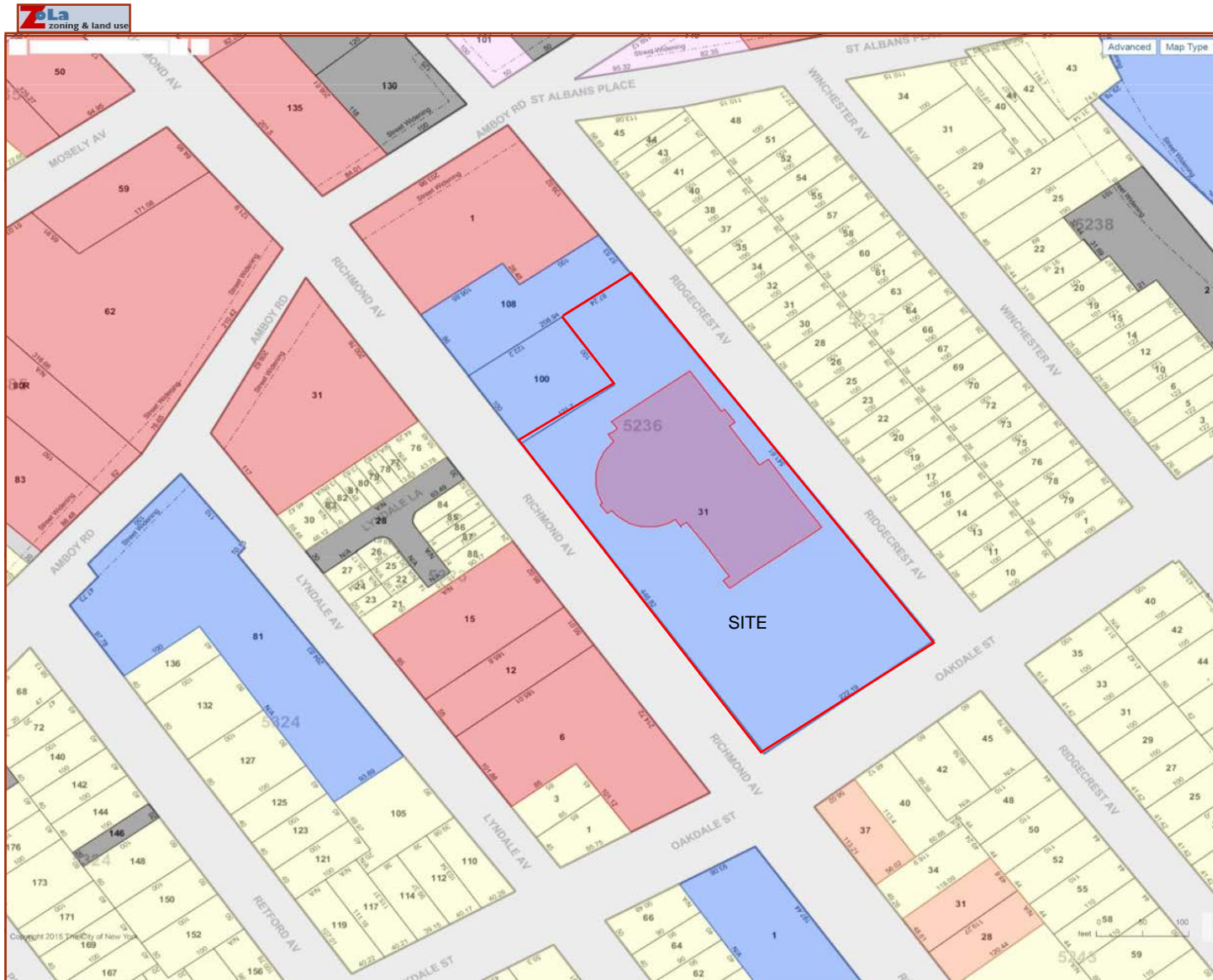
- RESIDENTIAL MANUFACTURING COMMERCIAL PARK/FOREST/OPEN SPACE OTHER, specify:
Community Facility

SITE LOCATION MAP



SITE

LAND USE MAP:



- 1 & 2 Family Residential
- Multi-family Residential
- Mixed Use
- Open space & outdoor recreation
- Commercial
- Institutions
- Industrial
- Parking
- Transportation / Utilities
- Vacant Lots

400' RADIUS MAP

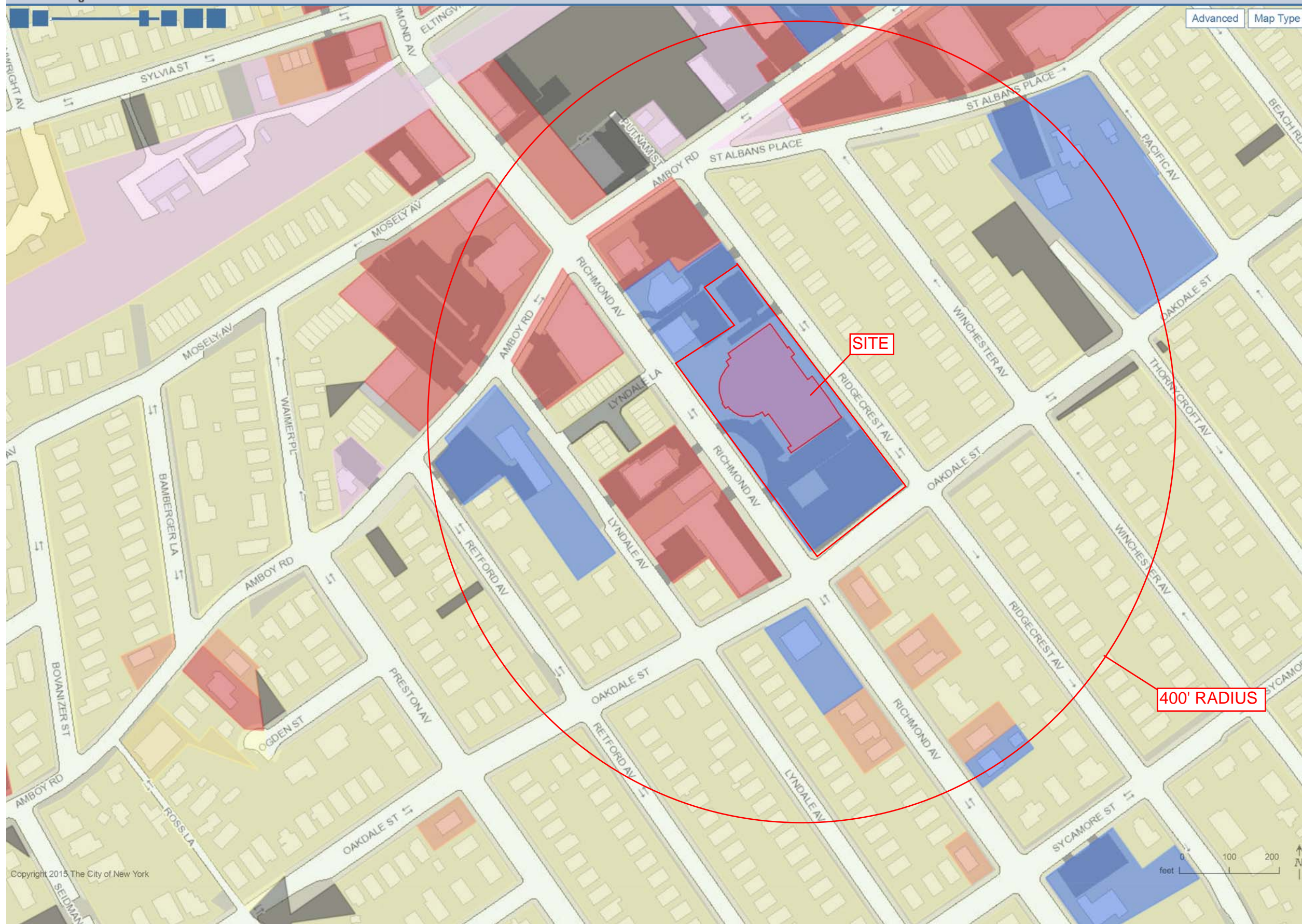
June 12, 2015



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Advanced Map Type

Search for a Location

Searched Locations

3939 RICHMOND AVENUE
STATEN ISLAND 10312

Hide Additional Information...

- Zoning

Zoning Information:

Borough: Staten Island Block: 5236 Lot: 31

Zoning: [R3-2](#)

Special Purpose District:
Special South Richmond Development District

Zoning Map: [33c](#)

Historical Zoning Maps: [33c](#)

Department of City Planning, Zoning Database (updated monthly)

- Additional Zoning Information

- Building & Property Information

Show Zoning & Related Data on Map



ZONING MAP

THE NEW YORK CITY PLANNING COMMISSION

Major Zoning Classifications:

The number(s) and/or letter(s) that follows on R, C or M Districts and Special Purpose Districts indicate the zoning controls as described in the text of the Zoning Resolution.

- R - RESIDENTIAL DISTRICT
- C - COMMERCIAL DISTRICT
- M - MANUFACTURING DISTRICT
- SPECIAL PURPOSE DISTRICT
- AREA(S) REZONED

Effective Date(s) of Rezoning:

06-13-2012 C 110077 ZMR

Special Requirements:

For a list of lots subject to CEQR environmental requirements, see APPENDIX C.
 For a list of lots subject to "D" restrictive declarations, see APPENDIX D.
 For Inclusionary Housing designated areas on this map, see APPENDIX F.

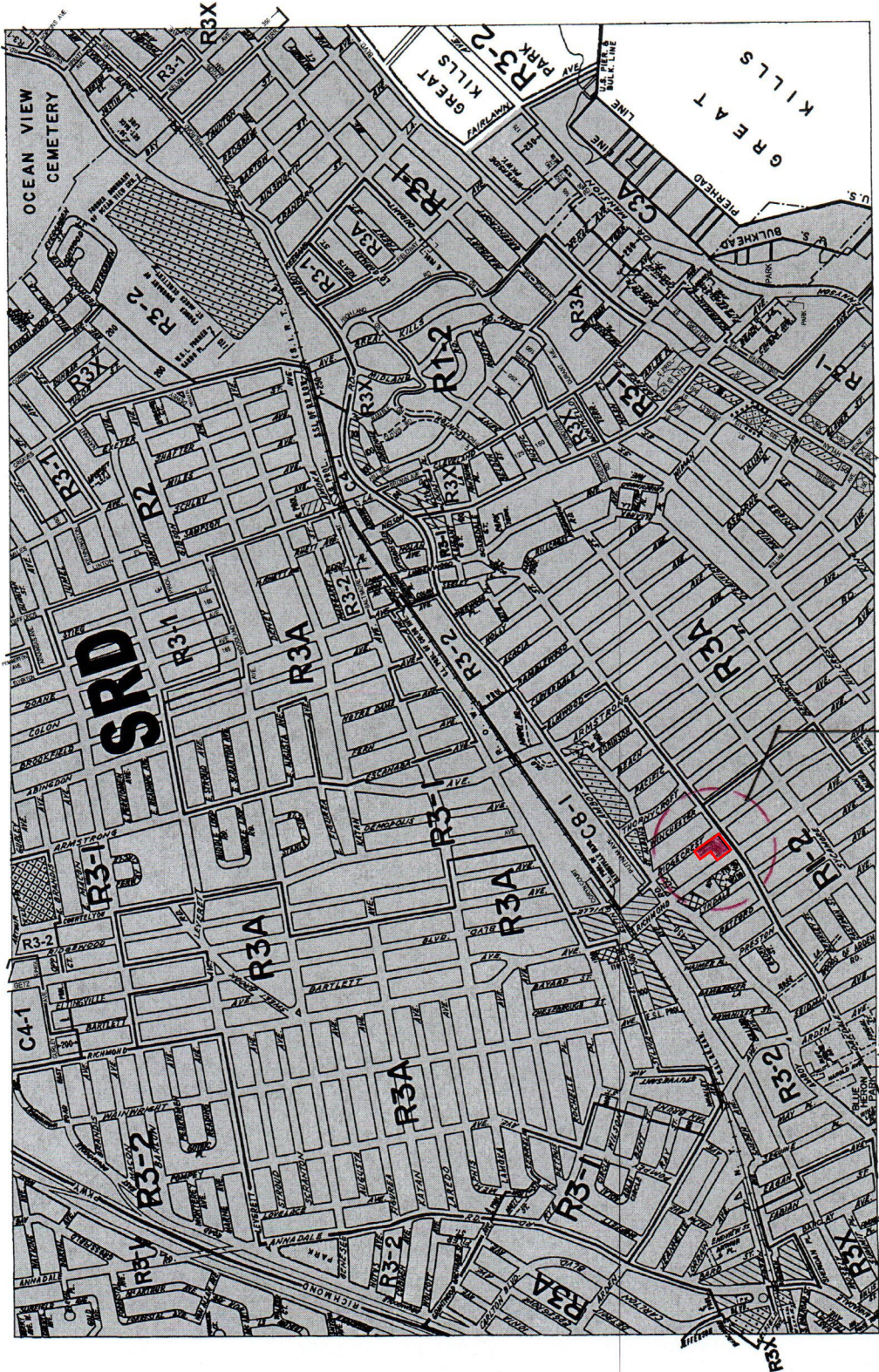


MAP KEY

26b	26d	27b
33a	33c	34a
33b	33d	34b

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NOTE: Zoning information as shown on this map is subject to change. For the most up-to-date zoning information for the map, visit the Zoning section of the Department of City Planning website at www.dcp.nyc.gov. For more information, contact the Zoning Information Desk at (212) 720-5281.



- C1-1
- C1-2
- C1-3
- C1-4
- C1-5
- C2-1
- C2-2
- C2-3
- C2-4
- C2-5

NOTE: Where no dimensions for zoning district boundaries appear on the zoning maps, such dimensions are determined in Article VII, Chapter 6 (Location of District Boundary) of the Zoning Resolution.

SITE



NYC TAX MAP

FINANCE
NEW YORK
MAYOR'S OFFICE
COMMISSIONER



NYC Digital Tax Map

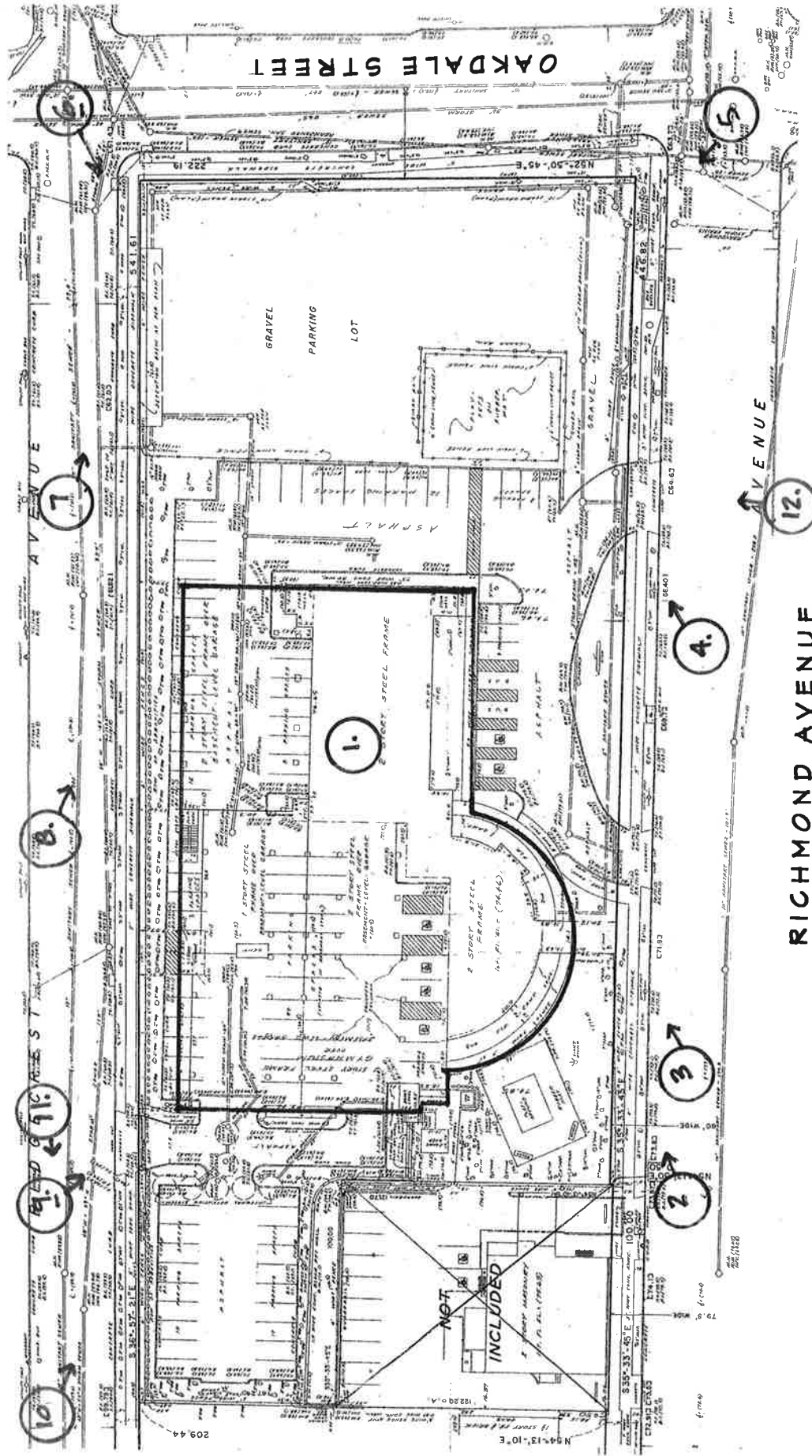
Effective Date : 12-06-2008 18:52:12
End Date : Current
Staten Island Block: 5236

Legend

- Streets
- Miscellaneous Text
- § Possession Hooks
- Boundary Lines
- § Lot Face Possession Hooks
- Regular
- Underwater
- Yellow Tax Lot Polygon
- Blue Condo Number
- Light Blue Tax Block Polygon



RIDGECREST AVENUE



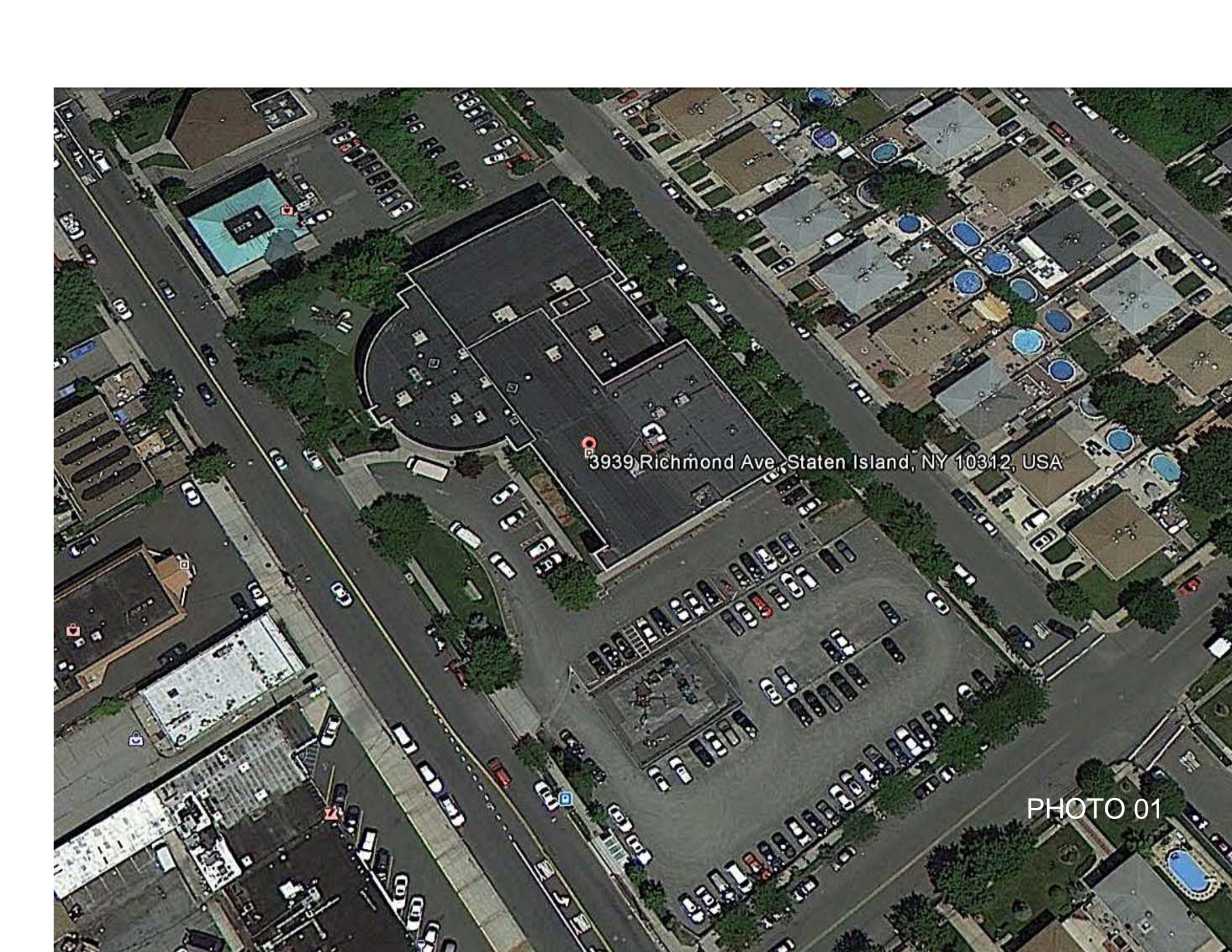
RICHMOND AVENUE

PHOTO LEGEND

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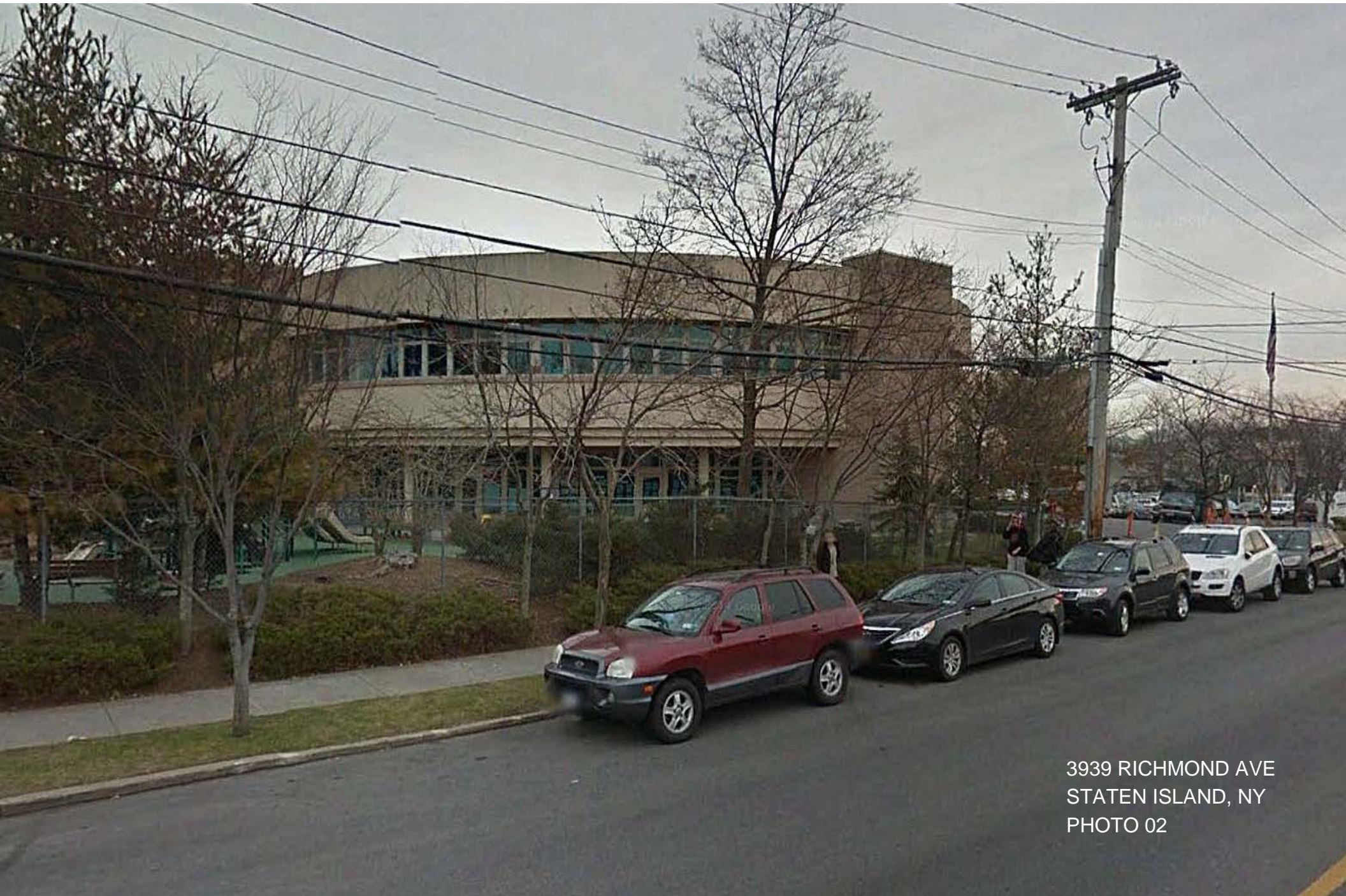
Staten Island YMCA
 3939 Richmond Avenue
 Staten Island, NY 10312





3939 Richmond Ave, Staten Island, NY 10312, USA

PHOTO 01



3939 RICHMOND AVE
STATEN ISLAND, NY
PHOTO 02



3939 RICHMOND AVE
STATEN ISLAND, NY
PHOTO 03



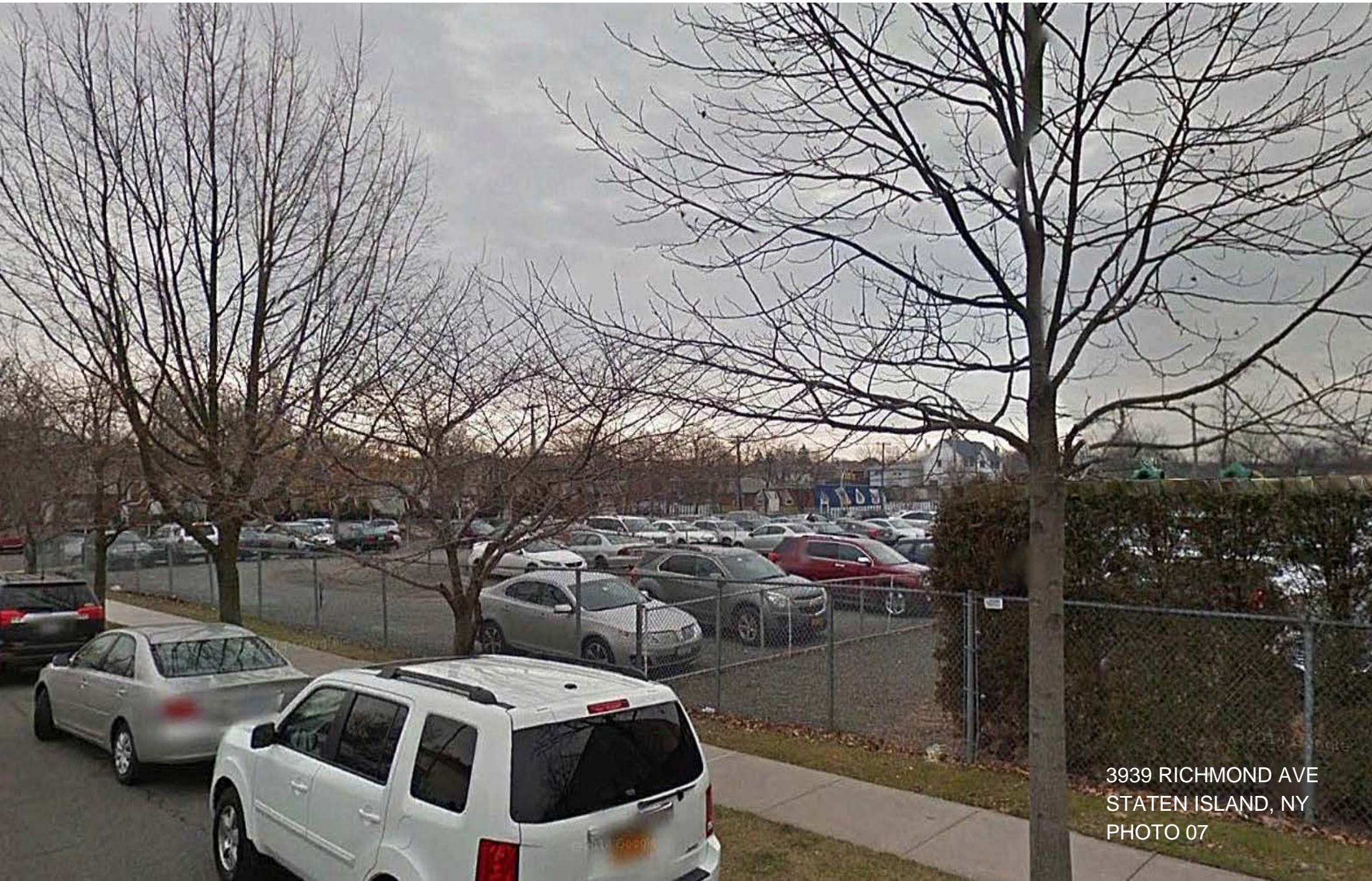
3939 RICHMOND AVE
STATEN ISLAND, NY
PHOTO 05



3939 RICHMOND AVE
STATEN ISLAND, NY
PHOTO 04



3939 RICHMOND AVE
STATEN ISLAND, NY
PHOTO 06



3939 RICHMOND AVE
STATEN ISLAND, NY
PHOTO 07



3939 RICHMOND AVE
STATEN ISLAND, NY
PHOTO 09



3939 RICHMOND AVE
STATEN ISLAND, NY
PHOTO 08



3939 RICHMOND AVE
STATEN ISLAND, NY
PHOTO 10



3939 RICHMOND AVE
STATEN ISLAND, NY
PHOTO 11



3939 RICHMOND AVE
STATEN ISLAND, NY
PHOTO 12

Staten Island YMCA South Shore Branch

Parking Lot Expansion

3939 Richmond Avenue
Staten Island, NY 10312

SCOPE OF WORK

THE STATEN ISLAND YMCA IS SEEKING APPROVAL TO CONVERT THE EXISTING TEMPORARY OVERFLOW PARKING AREA ATTACHED TO THE SOUTH SHORE FACILITY INTO 75 ADDITIONAL PERMANENT PARKING SPACES. THERE ARE CURRENTLY 107 PARKING SPACES LISTED ON THE CERTIFICATE OF OCCUPANCY. THE NEW PROPOSED TOTAL IS 182 PARKING SPACES.

GENERAL DATA

BLOCK	5236
LOT NO.	31
HOUSE NO.	3939
MAP NO.	33C
ZONE	R3-2
USE GROUP	4
COMMUNITY BOARD	503
FIRE DISTRICT	NO
DEC FRESHWATER WETLANDS	NO
WATERFRONT	NO
FLOOD HAZARD	NO
'LL' AREA	NO
NATURAL AREA	NO
HILLSIDE	NO
SPECIAL SOUTH RICHMOND SPECIAL DISTRICT	YES
PARK STREET	NO
ARTERIAL STREET (RICHMOND AVENUE)	YES
DOS	NO
WATERFRONT ESPLANADE	NO

ZONING DATA

ALL ZONING INFORMATION INDICATES EXISTING CONDITIONS. THERE IS NO NEW FLOOR AREA OR LOT COVERAGE BEING ADDED UNDER THIS APPLICATION.

LOT AREA (SF)	105,126
LOT COVERAGE (%)	23
CELLAR FLOOR AREA (SF) NOT INCL'D	7,028.00
1 ST FLOOR AREA (SF)	24,585.00
2 ND FLOOR AREA (SF)	20,850.00
TOTAL FLOOR AREA (SF)	50,435.00

TREE LEGEND:

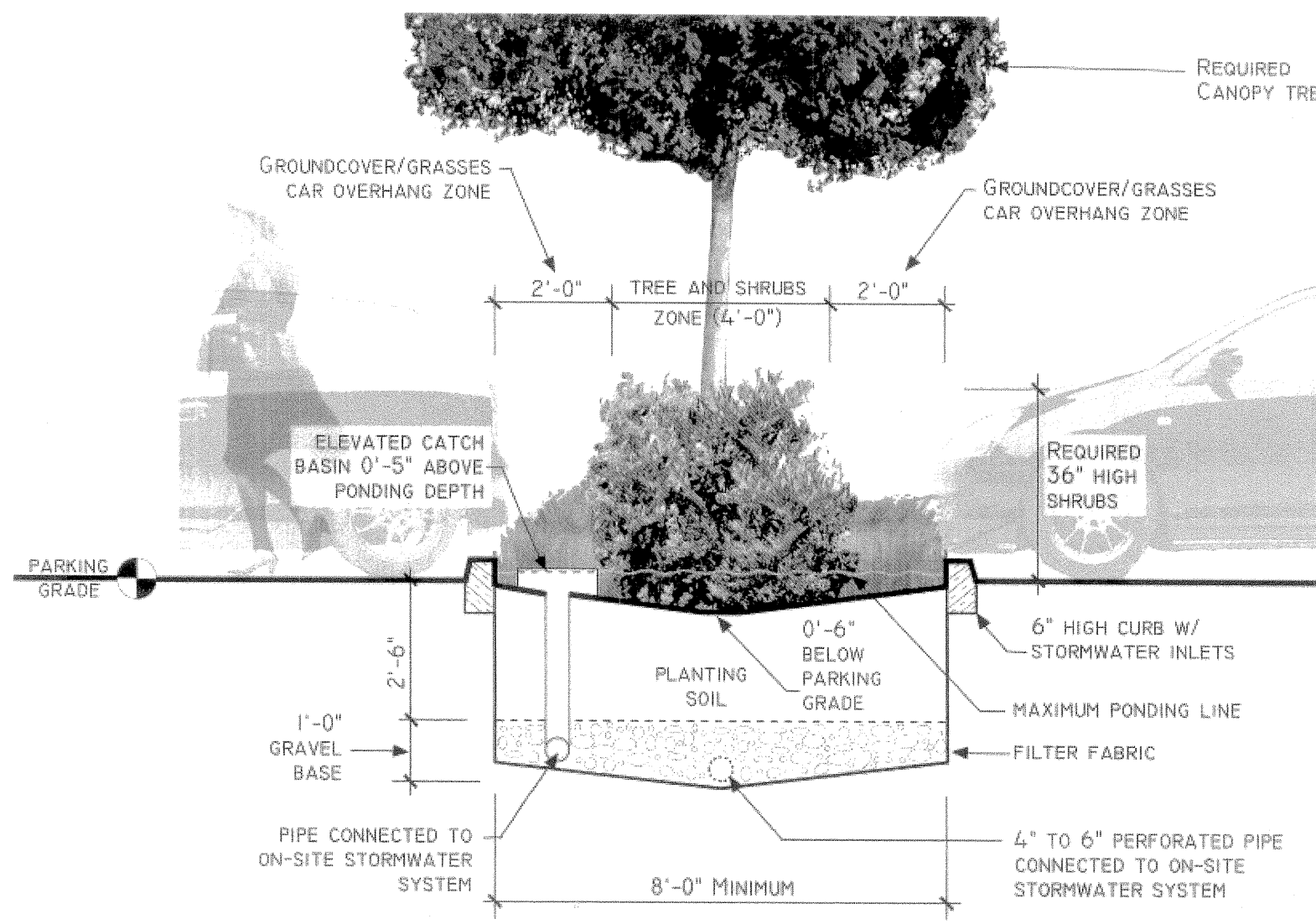
- NEW PARKING LOT TREE TO BE PLANTED (Symbol with 'L')
- NEW PERIMETER TREE TO BE PLANTED (Symbol with 'P')

NOTE: ALL EXISTING SIDEWALK TREES TO REMAIN

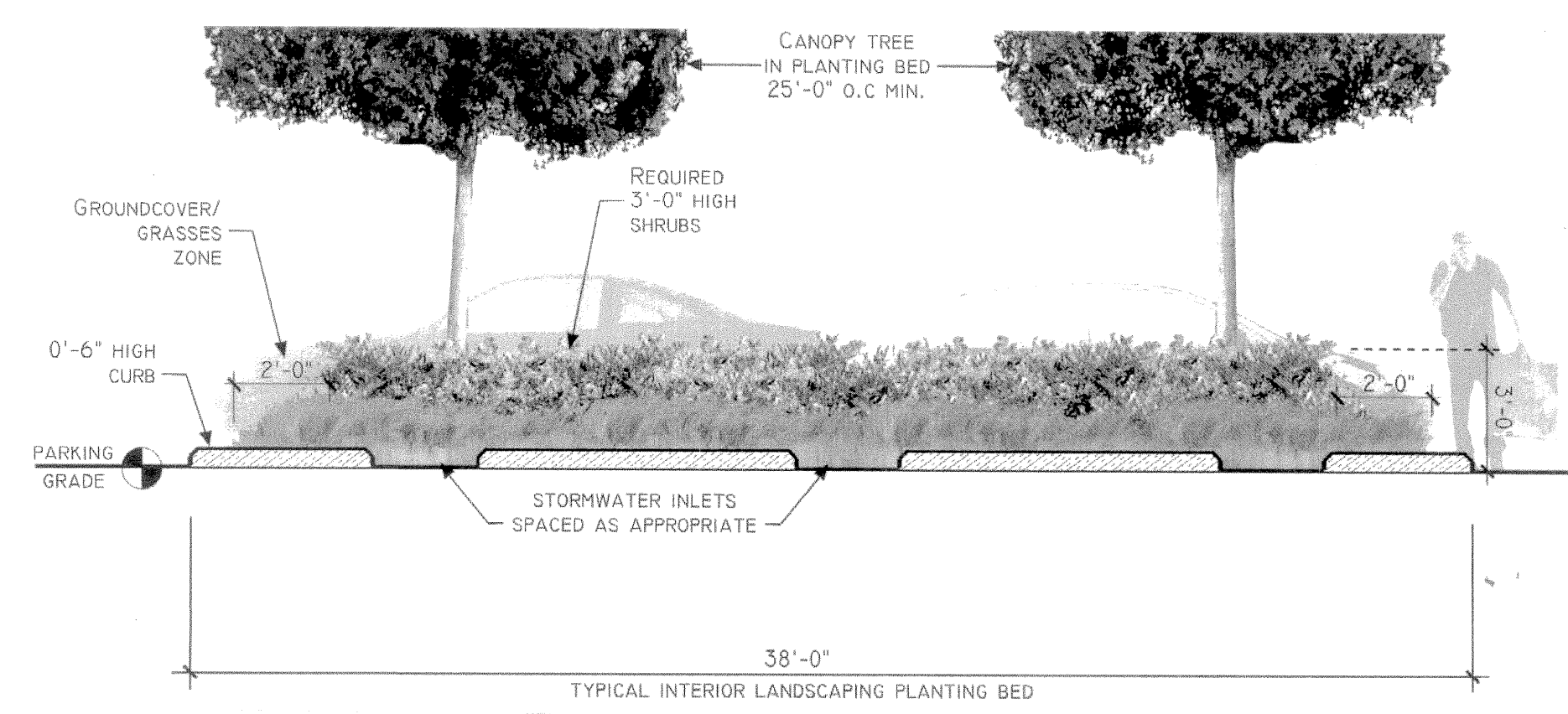
GRADE LEGEND

LEGAL GRADE	[0.00]
EXISTING GRADE	(0.00)
PROPOSED GRADE	<0.00>

ZONING SECTION	TITLE	REQUIREMENT	EXISTING CONDITIONS	PROPOSED	REMARKS
25-31	Minimum Req'd. Parking	107	107	182	Complies. Parking calculated as per regulations. 75 new additional voluntary parking spaces are proposed
25-62	Size, Location & Maneuverability	18'x8'-6" min. dimensions of a space; 22' min. travel lane	18'x9' w/22' lane	18'x9' w/22' lane	Complies
25-64	Restriction on Use of Open Space for Parking	50% of lot area not covered by buildings containing residences	N/A	N/A	N/A.
25-66 (b)	Screening	Min. 4' densely planted strip	wooded buffer	wooded buffer	Complies
25-811	Enclosed Bicycle Parking	1 space per 10,000 s.f. gross - Use Group 4	N/A	N/A	N/A
25-812 (a)	Unenclosed Bicycle Parking	1 space for every 10 automobile spaces up to 200, then 1 for every 100	11 required for 107 autospaces (none currently exist)	18 required for 181 auto spaces	Bicycle racks with minimum capacity of 18 spaces shall be provided where indicated on Site Plan
36-58	Size, Location & Maneuverability	18'x8'-6" min. space; 22' min. travel lane	18'x9' w/22' lane	18'x9' w/22' lane	Complies
107-472	Maximum Size of Group parking in Special S. Richmond Dev. Dist.	30 spaces / C.P.C. authorization to exceed	107	182	C.P.C. Authorization requested pursuant to Z.R. 107-68
25-67	Parking lot Landscaping	Parking lots containing more than 18 spaces to comply with Z.R. 37-90	N/A	N/A	Refers to and complies with Section 37-90: Parking Lots
37-421	Perimeter Landscaping	7 ft. planting strip	7 ft. planting strip	7 ft. planting strip	Complies
37-421(a)	Grading, Drainage & Soil	Grading for stormwater to collect and drain through perimeter landscaping	None	Provided for 472 lineal feet of frontage border	Request Authorization for portions where grading does not flow to the perimeter landscaping pursuant to Z.R. 107-68
37-421(b) (1)	Plantings - Parking Lot Frontage	First 2 ft. mulched & planted shrubs at 24" o.c.	None	Complies	New Plantings provided as indicated on Site Plan
37-421(b) (2)	Sidewalk Frontage	None	None	Complies	Maintain shrubs at max. height of 3' as noted on Site Plan
37-421(b)(3)	Trees /Perimeter Landscaping	1- 2" cal. tree per 25' of parking frontage	Existing dense wooded area /complies	472 ft. new pkg frontage requires 19 trees	Complies as indicated on Site Plan
37-421(c)	Obstructions	Open fences in Perimeter Landscaping not to exceed 4' in height	Existing 6' high iron fence - a long sidewalk	New 4' high aluminum fencing	Complies
37-422	Interior parking Landscaping	1- 3" caliper tree per 8 open parking spaces	107 existing spaces (61 under the building/46 open) 46/8= 6 trees	121 open spaces 121/8= 15 trees	Complies
37-422 (a) (1)	End row planted islands	End rows of 5+ spaces to have planted island	Complies	Complies	Complies
37-422 (a) (2)	Intermediate planted islands	No more than 15 spaces between planted islands	Complies	Complies	Complies
37-422 (a) (3)	Interior continuous island for parking lots greater than 150,000 s.f.	Every other row to abut perimeter or interior continuous island	Open Parking Less than 150,000 SF	N/A	N/A.
37-422 (b)	Interior Landscaping Islands drainage	Islands to collect storm drainage	Island do not drain per 37-422(b).	Proposed (new) parking areas comply	Request Authorization to waive the existing areas pursuant to Z.R. 107-68
37-422 (c)	Plantings	First 2 ft. from curbs mulched & planted w/groundcover; shrubs at 24" o.c. max	Islands planted with trees & grass only	Proposed (new) parking areas comply	Request Authorization to waive the existing areas pursuant to Z.R. 107-68
37-94	Refuse Storage	Min. 50 ft. from street; Screened with 6 ft. high masonry wall & opaque gate	Complies	N/A	Complies
107-31	Modifications of Existing Topography	Not more than 2' cut or fill, nor modifications resulting in destruction of trees 6" cal.	Complies	Complies	Complies.
107-321	Removal of trees 6" caliper or greater	Require CPC Authorization or Dept. of Buildings approval	N/A	N/A	N/A.
107-322	Tree Requirements - Lot	1 tree min. 3" cal. Per 1,000 s.f. lot area = 105 req'd	81 Trees	109 Trees	Complies
107-483 (a)	Parking lot landscaping	1- 3" caliper tree per 4 open pkg. spaces. 121/4=30 trees	81 Trees	109 Trees	Complies
107-483 (a)	Lots in excess of 30 cars	Min. of 50% of req'd. trees shall be in islands. 30/2=15	5 Trees	15 Trees	Complies
107-483 (b)	Perimeter Screening from adjoining zoning lots	4' wide strip densely planted with shrubs	Min. 7' wooded buffer exists	Complies	Complies



**SECTION A-A
THROUGH INTERIOR LANDSCAPING**

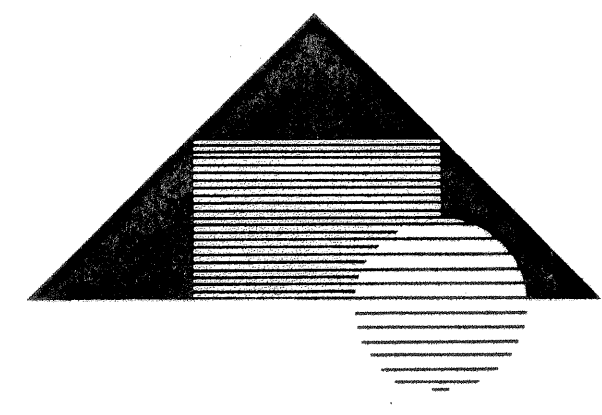


**ELEVATION '1'
@ INTERIOR LANDSCAPING**

Revisions
REV. 01 - JUNE 12, 2015

Seal

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Architect



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mabala@msn.com

Project

South Shore YMCA
3939 Richmond Avenue
Staten Island, NY 10312

Parking Lot Expansion

Drawing Status

**Zoning Data &
Notes**

Date: 4-11-15 Scale: See Plan

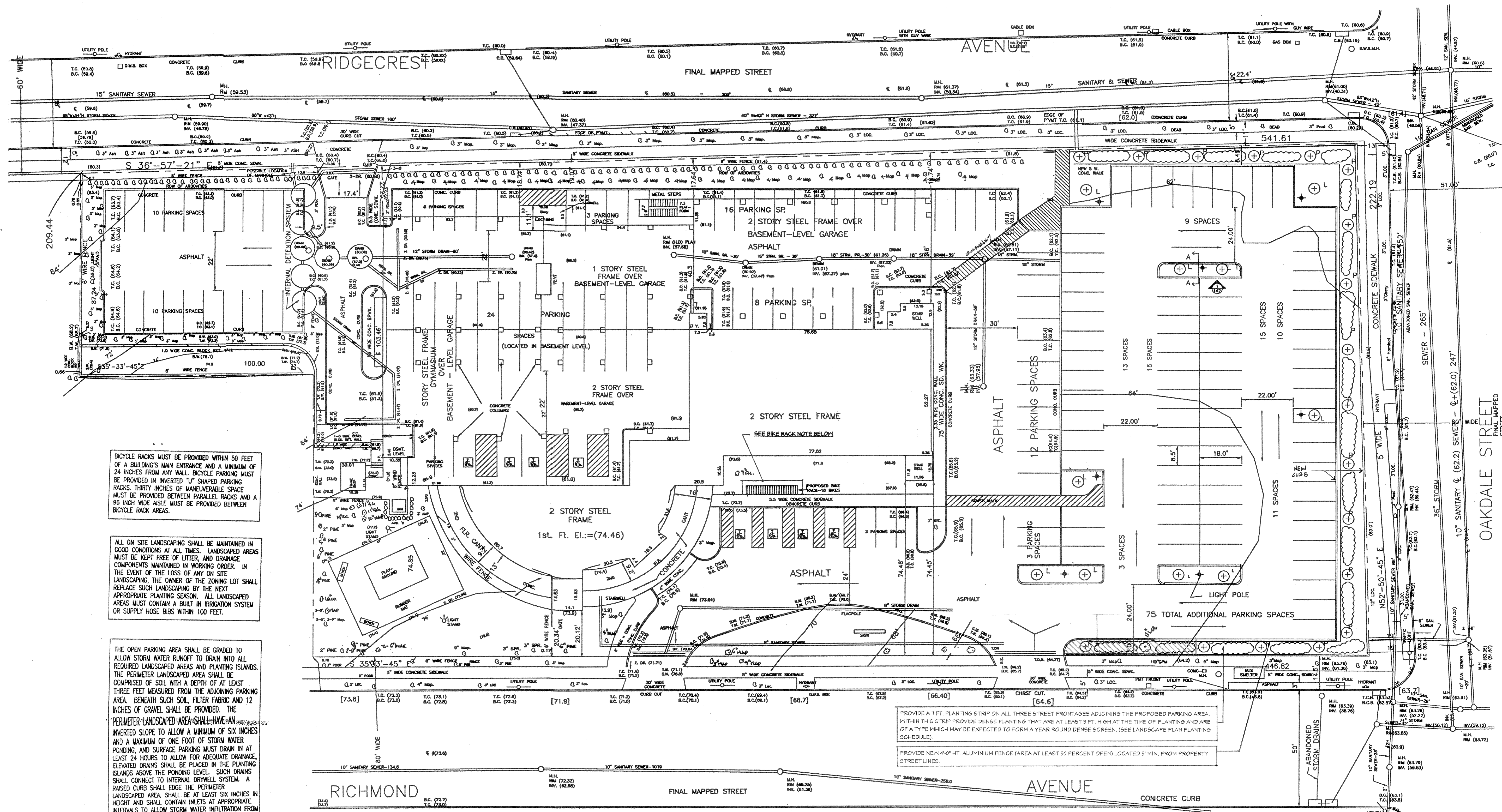
Drawn by: MAB Checked by:

Drawing

A-001.00

Sheet
CPC 01

Project Number
710 14



BICYCLE RACKS MUST BE PROVIDED WITHIN 50 FEET OF A BUILDING'S MAIN ENTRANCE AND A MINIMUM OF 24 INCHES FROM ANY WALL. BICYCLE PARKING MUST BE PROVIDED IN INVERTED 'U' SHAPED PARKING RACKS. THIRTY INCHES OF MANEUVERABLE SPACE MUST BE PROVIDED BETWEEN PARALLEL RACKS AND A 96 INCH WIDE AISLE MUST BE PROVIDED BETWEEN BICYCLE RACK AREAS.

ALL ON SITE LANDSCAPING SHALL BE MAINTAINED IN GOOD CONDITIONS AT ALL TIMES. LANDSCAPED AREAS MUST BE KEPT FREE OF LITTER, AND DRAINAGE COMPONENTS MAINTAINED IN WORKING ORDER. IN THE EVENT OF THE LOSS OF ANY ON SITE LANDSCAPING, THE OWNER OF THE ZONING LOT SHALL REPLACE SUCH LANDSCAPING BY THE NEXT APPROPRIATE PLANTING SEASON. ALL LANDSCAPED AREAS MUST CONTAIN A BUILT IN IRRIGATION SYSTEM OR SUPPLY HOSE BIBS WITHIN 100 FEET.

THE OPEN PARKING AREA SHALL BE GRADED TO ALLOW STORM WATER RUNOFF TO DRAIN INTO ALL REQUIRED LANDSCAPED AREAS AND PLANTING ISLANDS. THE PERIMETER LANDSCAPED AREA SHALL BE COMPRISSED OF SOIL WITH A DEPTH OF AT LEAST THREE FEET MEASURED FROM THE ADJOINING PARKING AREA. BENEATH SUCH SOIL, FILTER FABRIC AND 12 INCHES OF GRAVEL SHALL BE PROVIDED. THE PERIMETER LANDSCAPED AREA SHALL HAVE AN INVERTED SLOPE TO ALLOW A MINIMUM OF SIX INCHES AND A MAXIMUM OF ONE FOOT OF STORM WATER PONDING, AND SURFACE PARKING MUST DRAIN IN AT LEAST 24 HOURS TO ALLOW FOR ADEQUATE DRAINAGE. ELEVATED DRAINS SHALL BE PLACED IN THE PLANTING ISLANDS ABOVE THE PONDING LEVEL. SUCH DRAINS SHALL CONNECT TO INTERNAL DRYWELL SYSTEM. A RAISED CURB SHALL EXIST THE PERIMETER LANDSCAPED AREA. SHALL BE AT LEAST SIX INCHES IN HEIGHT AND SHALL CONTAIN INLETS AT APPROPRIATE INTERVALS TO ALLOW STORM WATER INFILTRATION FROM THE OPEN PARKING AREA.

THE FIRST TWO FEET OF THE PLANTING ISLAND FRONTING THE OPEN PARKING AREA SHALL BE COMPRISED OF MULCH AND DENSELY PLANTED WITH GROUND COVER ABOVE JUTE MESH TO STABILIZE THE INVERTED SLOPE. THE REMAINDER OF THE PERIMETER LANDSCAPED AREA SHALL BE DENSELY PLANTED WITH SHRUBS AT A DISTANCE OF 24 INCHES ON CENTER AND MAINTAINED AT A MAXIMUM HEIGHT OF THREE FEET ABOVE THE SURFACE OF THE ADJOINING PUBLIC SIDEWALK. ONE THREE-INCH CALIPER TREE SHALL BE PROVIDED FOR EVERY 25 FEET OF OPEN PARKING AREA STREET FRONTAGE. SUCH PERIMETER TREES SHALL BE STAGGERED WHENEVER POSSIBLE WITH STREET TREES, BUT IN NO EVENT SHALL PERIMETER TREES BE PLANTED CLOSER THAN 15 FEET ON CENTER OR WITHIN THREE FEET TO A PERIMETER SCREENING AREA CURB. FURTHERMORE, A RADIUS DISTANCE OF AT LEAST 20 FEET SHALL BE MAINTAINED BETWEEN TRUNKS OF PERIMETER TREES AND STREET TREES.

SITE PLAN

1 INCH = 20'-0"

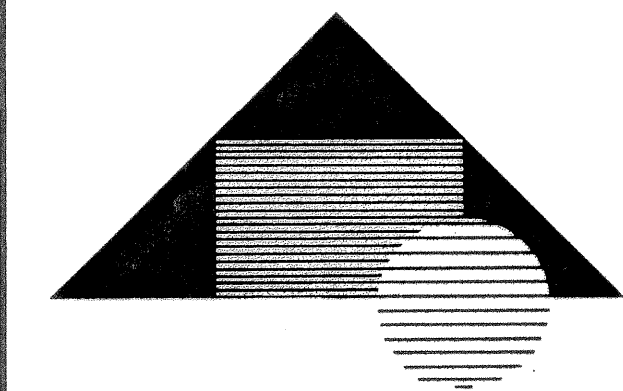
NOTE:
1. SEE GRADING & DRAINAGE PLAN FOR SITE GRADING, DRAINAGE AND BIO SWALE INFORMATION

PAVEMENT MARKINGS: FURNISH AND APPLY PAINT ON PAVEMENT SURFACES, IN THE FORM OF PARKING BAYS, CROSSWALKS, AND OTHER DETAIL PAVEMENT MARKINGS, IN ACCORDANCE WITH THE DETAILS AS SHOWN OR AS PRESCRIBED BY THE YMCA.

BIKE RACK INSTALLATION.
PROVIDE 4 INCH THICK CONCRETE SLAB ON 4-6 INCH GRAVEL BASE. MOUNT BIKE RACK AS PER MANUFACTURERS SPECIFICATIONS. NOTE: THE YMCA HAS A BIKE RACK IN STORAGE THAT THEY WANT INSTALLED IN THIS LOCATION IN LIEU OF THE ITEM SPECIFIED. GO TO EXAMINE THE STORED BIKE RACK AND ACCOMMODATE THIS ITEM FOR INSTALLATION. IF IT IS DETERMINED THAT THERE IS A PROBLEM WITH THE ITEM AND IT CANNOT BE INSTALLED, THE GC IS TO NOTIFY AND DISCUSS WITH THE PROPERTY MANAGER AND AMAT HIS DIRECTION.

Revisions
REV. 01 - JUNE 12, 2015

Michael A. Biagioli, AIA
Architect



4864 Arthur Kill Road, Suite 300
Staten Island, NY 10309
718.317.9600
mabai@msn.com

South Shore YMCA
3434 Richmond Avenue
Staten Island, NY 10312
Parking Lot Expansion

Drawing Status

Site Plan

Date April 1, 2014 Scale

Drawn by MAB Checked by

Drawing

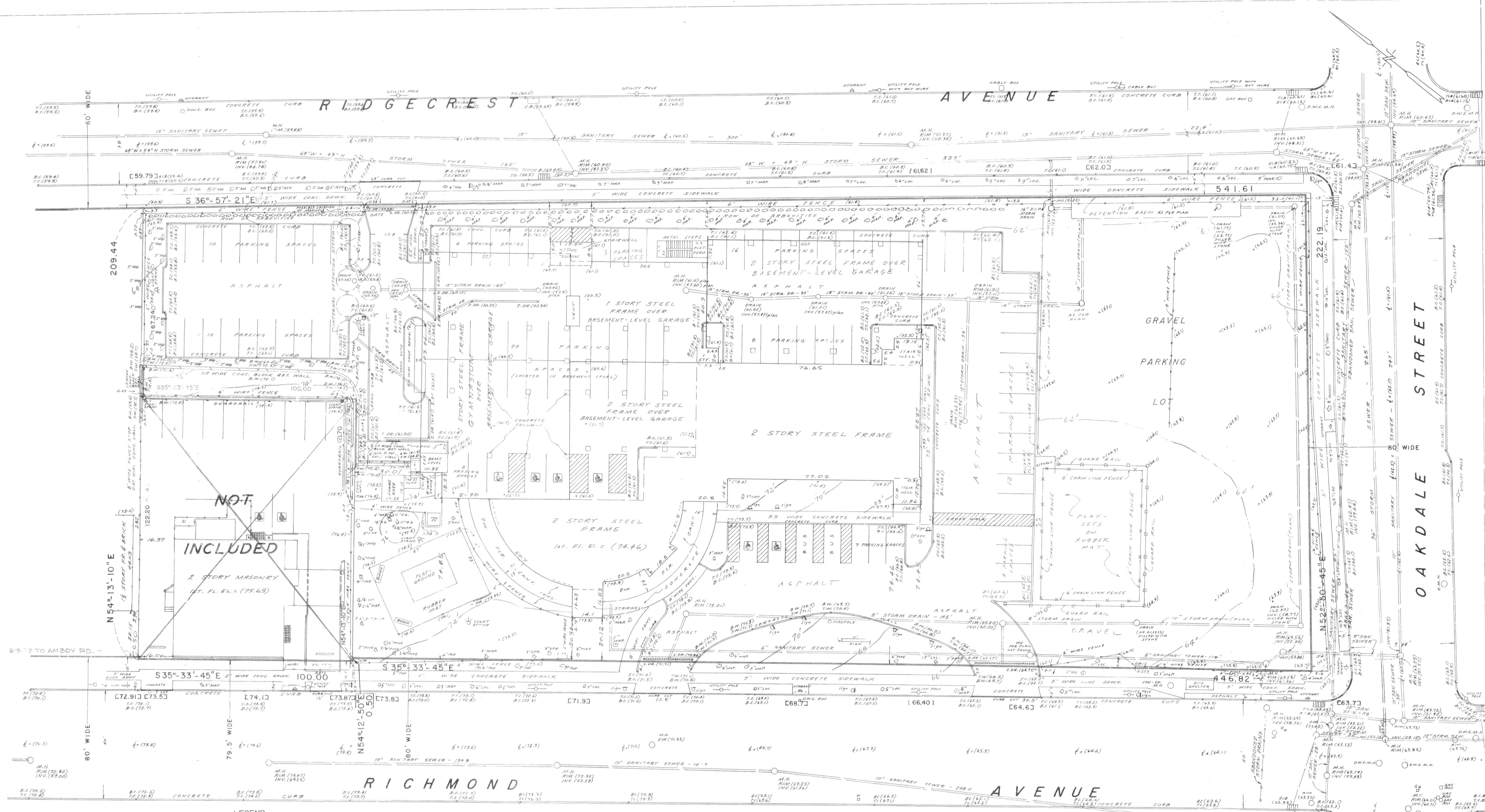
A-002.00

Sheet

CPC 02

Project Number

710 14



NOT INCLUDED
2 STORY MASONRY
1st. Fl. El. = (75.69)

TOTAL AREA = 105,126 Sq. Ft.

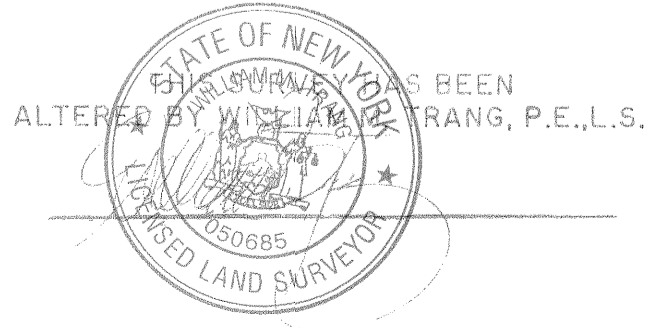
LEGEND

LEGAL GRADE	(57.4)
EXISTING ELEVATION	(58.4)
BUILDING	(Symbol)
WALLS	(Symbol)
FENCE	(Symbol)
SEWER	(Symbol)
WATER	(Symbol)
THEORETICAL CURB LINE	(Symbol)
CATCHBASIN	(Symbol)
FIRE HYDRANT	(Symbol)
VALVE	(Symbol)
TREE TYPE & TRUNK DIAMETER	(Symbol)
CURB & CURB CUT	(Symbol)
EASEMENT LINE & DIMENSION	40.00
LOT LINE & DIMENSION	40.00
RECORD STREET LINE & DIMENSION	200.00
WIDENING LINE & DIMENSION	100.00
ZONING DISTRICT DIVISION LINE	(Symbol)

MANHOLES

015 TRAFFIC SIGN	015 TRAFFIC SIGN	015 TRAFFIC SIGN	015 TRAFFIC SIGN
016 PARKING METER	016 PARKING METER	016 PARKING METER	016 PARKING METER
017 COAL CHUTE	017 COAL CHUTE	017 COAL CHUTE	017 COAL CHUTE
018 GUY WIRE	018 GUY WIRE	018 GUY WIRE	018 GUY WIRE
019 FIRE CALL BOX	019 FIRE CALL BOX	019 FIRE CALL BOX	019 FIRE CALL BOX
020 TRAFFIC CONTROL BOX	020 TRAFFIC CONTROL BOX	020 TRAFFIC CONTROL BOX	020 TRAFFIC CONTROL BOX
021 UNPAVED	021 UNPAVED	021 UNPAVED	021 UNPAVED
022 PAY PHONE	022 PAY PHONE	022 PAY PHONE	022 PAY PHONE
023 CONC. DRIVE	023 CONC. DRIVE	023 CONC. DRIVE	023 CONC. DRIVE
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- NOTES:**
- UNAUTHORIZED ALTERATION OR ADDITION TO A SURVEY BEARING A LICENSED LAND SURVEYOR'S SEAL IS A VIOLATION OF SECTION 7209, SUBDIVISION 2, OF THE NEW YORK STATE EDUCATION LAW.
 - ALL SUBSURFACE INFORMATION SHOWN HEREON IS TAKEN FROM VARIOUS MAPS AND UTILITY COMPANIES, AND IS NOT GUARANTEED FOR ACCURACY OR COMPLETENESS BY THE SURVEYOR.
 - SURVEYOR'S CONTRACTUAL LIABILITY SHALL RUN ONLY TO THOSE PARTIES TO WHOM THIS SURVEY IS CERTIFIED.
 - ONLY COPIES FROM THE ORIGINAL OF THIS SURVEY MARKED WITH AN ORIGINAL OF THE LAND SURVEYOR'S EMBOSSED SEAL SHALL BE CONSIDERED VALID TRUE COPIES.
 - THIS IS TO CERTIFY THAT THERE ARE NO STREAMS OR NATURAL WATERCOURSES ACROSS THE PROPERTY AS SHOWN ON THIS SURVEY.



SCALE: 1" = 20'
SURVEY # 32624-L
F.B. 799, 537/48
853/28
1008/68
D.C.
PRINT

TAX MAP INFORMATION: SEC. 23 VOL. 1
B.L.K. 5236 LOT 31

FILED MAP INFORMATION

BOROUGH OF STATEN ISLAND
CITY OF NEW YORK
CitiesVoice
V. J. H. & O. M. A. L.L.P.
14 VANDERBILT AVE.
L.I.N.Y. 448-7456

FINAL ELEVATIONS SHOWN THUS: ()
EXISTING ELEVATIONS SHOWN THUS: ()
ELEVATIONS REFER TO RICHMOND HIGH WATER DATUM.

COORDINATES AND BEARINGS ARE IN A SYSTEM ESTABLISHED BY THE U.S.C. & G.S. FOR THE BOROUGH OF RICHMOND.

LOCATION LOCATED 12-19-96
LOCATION 8-26-00
THIS SURVEY IS CERTIFIED TO

Only copies from the original of this survey, marked with an embossed seal of the land surveyor, are valid true copies.

(RB) CRD 11/04-12-14/ 4407-4463
 (RB) CRD 15/03-03-16/ 2016-2015

Part II: TECHNICAL ANALYSIS

INSTRUCTIONS: For each of the analysis categories listed in this section, assess the proposed project’s impacts based on the thresholds and criteria presented in the CEQR Technical Manual. Check each box that applies.

- If the proposed project can be demonstrated not to meet or exceed the threshold, check the “no” box.
- If the proposed project will meet or exceed the threshold, or if this cannot be determined, check the “yes” box.
- For each “yes” response, provide additional analyses (and, if needed, attach supporting information) based on guidance in the CEQR Technical Manual to determine whether the potential for significant impacts exists. Please note that a “yes” answer does not mean that an EIS must be prepared—it means that more information may be required for the lead agency to make a determination of significance.
- The lead agency, upon reviewing Part II, may require an applicant to provide additional information to support the Short EAS Form. For example, if a question is answered “no,” an agency may request a short explanation for this response.

	YES	NO
1. LAND USE, ZONING, AND PUBLIC POLICY: CEQR Technical Manual Chapter 4		
(a) Would the proposed project result in a change in land use different from surrounding land uses?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(b) Would the proposed project result in a change in zoning different from surrounding zoning?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(c) Is there the potential to affect an applicable public policy?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(d) If “yes,” to (a), (b), and/or (c), complete a preliminary assessment and attach.		
(e) Is the project a large, publicly sponsored project?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
o If “yes,” complete a PlaNYC assessment and attach.		
(f) Is any part of the directly affected area within the City’s Waterfront Revitalization Program boundaries ?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
o If “yes,” complete the Consistency Assessment Form .		
2. SOCIOECONOMIC CONDITIONS: CEQR Technical Manual Chapter 5		
(a) Would the proposed project:		
o Generate a net increase of 200 or more residential units?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
o Generate a net increase of 200,000 or more square feet of commercial space?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
o Directly displace more than 500 residents?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
o Directly displace more than 100 employees?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
o Affect conditions in a specific industry?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
3. COMMUNITY FACILITIES: CEQR Technical Manual Chapter 6		
(a) Direct Effects		
o Would the project directly eliminate, displace, or alter public or publicly funded community facilities such as educational facilities, libraries, hospitals and other health care facilities, day care centers, police stations, or fire stations?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(b) Indirect Effects		
o Child Care Centers: Would the project result in 20 or more eligible children under age 6, based on the number of low or low/moderate income residential units? (See Table 6-1 in Chapter 6)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
o Libraries: Would the project result in a 5 percent or more increase in the ratio of residential units to library branches? (See Table 6-1 in Chapter 6)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
o Public Schools: Would the project result in 50 or more elementary or middle school students, or 150 or more high school students based on number of residential units? (See Table 6-1 in Chapter 6)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
o Health Care Facilities and Fire/Police Protection: Would the project result in the introduction of a sizeable new neighborhood?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
4. OPEN SPACE: CEQR Technical Manual Chapter 7		
(a) Would the proposed project change or eliminate existing open space?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(b) Is the project located within an under-served area in the Bronx , Brooklyn , Manhattan , Queens , or Staten Island ?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
o If “yes,” would the proposed project generate more than 50 additional residents or 125 additional employees?	<input type="checkbox"/>	<input type="checkbox"/>
(c) Is the project located within a well-served area in the Bronx , Brooklyn , Manhattan , Queens , or Staten Island ?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
o If “yes,” would the proposed project generate more than 350 additional residents or 750 additional employees?	<input type="checkbox"/>	<input type="checkbox"/>
(d) If the project is located in an area that is neither under-served nor well-served, would it generate more than 200 additional residents or 500 additional employees?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

	YES	NO
5. SHADOWS: CEQR Technical Manual Chapter 8		
(a) Would the proposed project result in a net height increase of any structure of 50 feet or more?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(b) Would the proposed project result in any increase in structure height and be located adjacent to or across the street from a sunlight-sensitive resource?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
6. HISTORIC AND CULTURAL RESOURCES: CEQR Technical Manual Chapter 9		
(a) Does the proposed project site or an adjacent site contain any architectural and/or archaeological resource that is eligible for or has been designated (or is calendared for consideration) as a New York City Landmark, Interior Landmark or Scenic Landmark; that is listed or eligible for listing on the New York State or National Register of Historic Places; or that is within a designated or eligible New York City, New York State or National Register Historic District? (See the GIS System for Archaeology and National Register to confirm)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(b) Would the proposed project involve construction resulting in in-ground disturbance to an area not previously excavated?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(c) If "yes" to either of the above, list any identified architectural and/or archaeological resources and attach supporting information on whether the proposed project would potentially affect any architectural or archeological resources.		
7. URBAN DESIGN AND VISUAL RESOURCES: CEQR Technical Manual Chapter 10		
(a) Would the proposed project introduce a new building, a new building height, or result in any substantial physical alteration to the streetscape or public space in the vicinity of the proposed project that is not currently allowed by existing zoning?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(b) Would the proposed project result in obstruction of publicly accessible views to visual resources not currently allowed by existing zoning?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
8. NATURAL RESOURCES: CEQR Technical Manual Chapter 11		
(a) Does the proposed project site or a site adjacent to the project contain natural resources as defined in Section 100 of Chapter 11 ?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
o If "yes," list the resources and attach supporting information on whether the proposed project would affect any of these resources.		
(b) Is any part of the directly affected area within the Jamaica Bay Watershed ?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
o If "yes," complete the Jamaica Bay Watershed Form , and submit according to its instructions .		
9. HAZARDOUS MATERIALS: CEQR Technical Manual Chapter 12		
(a) Would the proposed project allow commercial or residential uses in an area that is currently, or was historically, a manufacturing area that involved hazardous materials?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(b) Does the proposed project site have existing institutional controls (e.g., (E) designation or Restrictive Declaration) relating to hazardous materials that preclude the potential for significant adverse impacts?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(c) Would the project require soil disturbance in a manufacturing area or any development on or near a manufacturing area or existing/historic facilities listed in Appendix 1 (including nonconforming uses)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(d) Would the project result in the development of a site where there is reason to suspect the presence of hazardous materials, contamination, illegal dumping or fill, or fill material of unknown origin?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(e) Would the project result in development on or near a site that has or had underground and/or aboveground storage tanks (e.g., gas stations, oil storage facilities, heating oil storage)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(f) Would the project result in renovation of interior existing space on a site with the potential for compromised air quality; vapor intrusion from either on-site or off-site sources; or the presence of asbestos, PCBs, mercury or lead-based paint?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(g) Would the project result in development on or near a site with potential hazardous materials issues such as government-listed voluntary cleanup/brownfield site, current or former power generation/transmission facilities, coal gasification or gas storage sites, railroad tracks or rights-of-way, or municipal incinerators?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(h) Has a Phase I Environmental Site Assessment been performed for the site?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
o If "yes," were Recognized Environmental Conditions (RECs) identified? Briefly identify:	<input type="checkbox"/>	<input type="checkbox"/>
10. WATER AND SEWER INFRASTRUCTURE: CEQR Technical Manual Chapter 13		
(a) Would the project result in water demand of more than one million gallons per day?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(b) If the proposed project located in a combined sewer area, would it result in at least 1,000 residential units or 250,000 square feet or more of commercial space in Manhattan, or at least 400 residential units or 150,000 square feet or more of commercial space in the Bronx, Brooklyn, Staten Island, or Queens?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(c) If the proposed project located in a separately sewered area , would it result in the same or greater development than the amounts listed in Table 13-1 in Chapter 13 ?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(d) Would the proposed project involve development on a site that is 5 acres or larger where the amount of impervious surface would increase?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(e) If the project is located within the Jamaica Bay Watershed or in certain specific drainage areas , including Bronx River, Coney Island Creek, Flushing Bay and Creek, Gowanus Canal, Hutchinson River, Newtown Creek, or Westchester Creek, would it involve development on a site that is 1 acre or larger where the amount of impervious surface would increase?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

	YES	NO
(f) Would the proposed project be located in an area that is partially sewerred or currently unsewerred?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(g) Is the project proposing an industrial facility or activity that would contribute industrial discharges to a Wastewater Treatment Plant and/or generate contaminated stormwater in a separate storm sewer system?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(h) Would the project involve construction of a new stormwater outfall that requires federal and/or state permits?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
11. SOLID WASTE AND SANITATION SERVICES: CEQR Technical Manual Chapter 14		
(a) Using Table 14-1 in Chapter 14 , the project's projected operational solid waste generation is estimated to be (pounds per week):		
o Would the proposed project have the potential to generate 100,000 pounds (50 tons) or more of solid waste per week?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(b) Would the proposed project involve a reduction in capacity at a solid waste management facility used for refuse or recyclables generated within the City?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
12. ENERGY: CEQR Technical Manual Chapter 15		
(a) Using energy modeling or Table 15-1 in Chapter 15 , the project's projected energy use is estimated to be (annual BTUs):		
(b) Would the proposed project affect the transmission or generation of energy?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
13. TRANSPORTATION: CEQR Technical Manual Chapter 16		
(a) Would the proposed project exceed any threshold identified in Table 16-1 in Chapter 16 ?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
(b) If "yes," conduct the screening analyses, attach appropriate back up data as needed for each stage and answer the following questions:		
o Would the proposed project result in 50 or more Passenger Car Equivalents (PCEs) per project peak hour?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
If "yes," would the proposed project result in 50 or more vehicle trips per project peak hour at any given intersection? **It should be noted that the lead agency may require further analysis of intersections of concern even when a project generates fewer than 50 vehicles in the peak hour. See Subsection 313 of Chapter 16 for more information.	<input type="checkbox"/>	<input type="checkbox"/>
o Would the proposed project result in more than 200 subway/rail or bus trips per project peak hour?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
If "yes," would the proposed project result, per project peak hour, in 50 or more bus trips on a single line (in one direction) or 200 subway trips per station or line?	<input type="checkbox"/>	<input type="checkbox"/>
o Would the proposed project result in more than 200 pedestrian trips per project peak hour?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
If "yes," would the proposed project result in more than 200 pedestrian trips per project peak hour to any given pedestrian or transit element, crosswalk, subway stair, or bus stop?	<input type="checkbox"/>	<input type="checkbox"/>
14. AIR QUALITY: CEQR Technical Manual Chapter 17		
(a) <i>Mobile Sources:</i> Would the proposed project result in the conditions outlined in Section 210 in Chapter 17 ?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
(b) <i>Stationary Sources:</i> Would the proposed project result in the conditions outlined in Section 220 in Chapter 17 ?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
o If "yes," would the proposed project exceed the thresholds in Figure 17-3, Stationary Source Screen Graph in Chapter 17 ? (Attach graph as needed)	<input type="checkbox"/>	<input type="checkbox"/>
(c) Does the proposed project involve multiple buildings on the project site?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(d) Does the proposed project require federal approvals, support, licensing, or permits subject to conformity requirements?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(e) Does the proposed project site have existing institutional controls (e.g., (E) designation or Restrictive Declaration) relating to air quality that preclude the potential for significant adverse impacts?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
15. GREENHOUSE GAS EMISSIONS: CEQR Technical Manual Chapter 18		
(a) Is the proposed project a city capital project or a power generation plant?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(b) Would the proposed project fundamentally change the City's solid waste management system?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(c) If "yes" to any of the above, would the project require a GHG emissions assessment based on the guidance in Chapter 18 ?	<input type="checkbox"/>	<input type="checkbox"/>
16. NOISE: CEQR Technical Manual Chapter 19		
(a) Would the proposed project generate or reroute vehicular traffic?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(b) Would the proposed project introduce new or additional receptors (see Section 124 in Chapter 19) near heavily trafficked roadways, within one horizontal mile of an existing or proposed flight path, or within 1,500 feet of an existing or proposed rail line with a direct line of site to that rail line?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(c) Would the proposed project cause a stationary noise source to operate within 1,500 feet of a receptor with a direct line of sight to that receptor or introduce receptors into an area with high ambient stationary noise?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(d) Does the proposed project site have existing institutional controls (e.g., (E) designation or Restrictive Declaration) relating to noise that preclude the potential for significant adverse impacts?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
17. PUBLIC HEALTH: CEQR Technical Manual Chapter 20		
(a) Based upon the analyses conducted, do any of the following technical areas require a detailed analysis: Air Quality;	<input type="checkbox"/>	<input checked="" type="checkbox"/>

	YES	NO
Hazardous Materials; Noise?		
(b) If "yes," explain why an assessment of public health is or is not warranted based on the guidance in Chapter 20 , "Public Health." Attach a preliminary analysis, if necessary.		
18. NEIGHBORHOOD CHARACTER: CEQR Technical Manual Chapter 21		
(a) Based upon the analyses conducted, do any of the following technical areas require a detailed analysis: Land Use, Zoning, and Public Policy; Socioeconomic Conditions; Open Space; Historic and Cultural Resources; Urban Design and Visual Resources; Shadows; Transportation; Noise?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(b) If "yes," explain why an assessment of neighborhood character is or is not warranted based on the guidance in Chapter 21 , "Neighborhood Character." Attach a preliminary analysis, if necessary.		
19. CONSTRUCTION: CEQR Technical Manual Chapter 22		
(a) Would the project's construction activities involve:		
o Construction activities lasting longer than two years?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
o Construction activities within a Central Business District or along an arterial highway or major thoroughfare?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
o Closing, narrowing, or otherwise impeding traffic, transit, or pedestrian elements (roadways, parking spaces, bicycle routes, sidewalks, crosswalks, corners, etc.)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
o Construction of multiple buildings where there is a potential for on-site receptors on buildings completed before the final build-out?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
o The operation of several pieces of diesel equipment in a single location at peak construction?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
o Closure of a community facility or disruption in its services?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
o Activities within 400 feet of a historic or cultural resource?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
o Disturbance of a site containing or adjacent to a site containing natural resources?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
o Construction on multiple development sites in the same geographic area, such that there is the potential for several construction timelines to overlap or last for more than two years overall?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(b) If any boxes are checked "yes," explain why a preliminary construction assessment is or is not warranted based on the guidance in Chapter 22 , "Construction." It should be noted that the nature and extent of any commitment to use the Best Available Technology for construction equipment or Best Management Practices for construction activities should be considered when making this determination.		

20. APPLICANT'S CERTIFICATION

I swear or affirm under oath and subject to the penalties for perjury that the information provided in this Environmental Assessment Statement (EAS) is true and accurate to the best of my knowledge and belief, based upon my personal knowledge and familiarity with the information described herein and after examination of the pertinent books and records and/or after inquiry of persons who have personal knowledge of such information or who have examined pertinent books and records.

Still under oath, I further swear or affirm that I make this statement in my capacity as the applicant or representative of the entity that seeks the permits, approvals, funding, or other governmental action(s) described in this EAS.

APPLICANT/REPRESENTATIVE NAME

Michael A. Biagioli, AIA

DATE

June 12, 2015


SIGNATURE



PLEASE NOTE THAT APPLICANTS MAY BE REQUIRED TO SUBSTANTIATE RESPONSES IN THIS FORM AT THE DISCRETION OF THE LEAD AGENCY SO THAT IT MAY SUPPORT ITS DETERMINATION OF SIGNIFICANCE.

Part III: DETERMINATION OF SIGNIFICANCE (To Be Completed by Lead Agency)

INSTRUCTIONS: In completing Part III, the lead agency should consult 6 NYCRR 617.7 and 43 RCNY § 6-06 (Executive Order 91 or 1977, as amended), which contain the State and City criteria for determining significance.

1. For each of the impact categories listed below, consider whether the project may have a significant adverse effect on the environment, taking into account its (a) location; (b) probability of occurring; (c) duration; (d) irreversibility; (e) geographic scope; and (f) magnitude.	Potentially Significant Adverse Impact	
	YES	NO
IMPACT CATEGORY		
Land Use, Zoning, and Public Policy	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Socioeconomic Conditions	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Community Facilities and Services	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Open Space	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Shadows	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Historic and Cultural Resources	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Urban Design/Visual Resources	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Natural Resources	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Hazardous Materials	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Water and Sewer Infrastructure	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Solid Waste and Sanitation Services	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Energy	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Transportation	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Air Quality	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Greenhouse Gas Emissions	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Noise	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Public Health	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Neighborhood Character	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Construction	<input type="checkbox"/>	<input checked="" type="checkbox"/>
2. Are there any aspects of the project relevant to the determination of whether the project may have a significant impact on the environment, such as combined or cumulative impacts, that were not fully covered by other responses and supporting materials? If there are such impacts, attach an explanation stating whether, as a result of them, the project may have a significant impact on the environment.	<input type="checkbox"/>	<input checked="" type="checkbox"/>
3. Check determination to be issued by the lead agency:		
<input type="checkbox"/> Positive Declaration: If the lead agency has determined that the project may have a significant impact on the environment, and if a Conditional Negative Declaration is not appropriate, then the lead agency issues a <i>Positive Declaration</i> and prepares a draft Scope of Work for the Environmental Impact Statement (EIS).		
<input type="checkbox"/> Conditional Negative Declaration: A <i>Conditional Negative Declaration</i> (CND) may be appropriate if there is a private applicant for an Unlisted action AND when conditions imposed by the lead agency will modify the proposed project so that no significant adverse environmental impacts would result. The CND is prepared as a separate document and is subject to the requirements of 6 NYCRR Part 617.		
<input checked="" type="checkbox"/> Negative Declaration: If the lead agency has determined that the project would not result in potentially significant adverse environmental impacts, then the lead agency issues a <i>Negative Declaration</i> . The <i>Negative Declaration</i> may be prepared as a separate document (see template) or using the embedded Negative Declaration on the next page.		
4. LEAD AGENCY'S CERTIFICATION		
TITLE Deputy Director, Environmental Assessment & Review Division	LEAD AGENCY New York City Department of City Planning	
NAME Olga Abinader	DATE June 12, 2015	
SIGNATURE 		

ATTACHMENT 'A'
PROJECT DESCRIPTION

PROJECT SUMMARY

The applicant, The YMCA of Greater New York, is seeking an authorization from the City Planning Commission pursuant to Sect. ZR 107-68: Authorization of the Modification of Group Parking Facility and Access Regulations and Sect. 107-65: Modification of Existing Topography, in connection with a proposal by the applicant to formalize a temporary, approximately 28,000 square foot (sf) area containing 75 parking spaces and associated parking lot landscaping accessory to an existing community facility building on the project site (Block 5236, Lot 31) in the Eltingville neighborhood in the Borough of Staten Island, Community District 3.

I. PROJECT DESCRIPTION

A. ACTIONS NECESSARY TO FACILITATE THE PROPOSAL

The applicant, The YMCA of Greater New York, seeks a zoning authorization pursuant to New York City Zoning Resolution (ZR) section 107-68 (“Authorization for Modification of a Group Parking Facility and Access Regulations”) affecting a site (Block 5236, Lot 31, the “project site”) located in the Eltingville neighborhood of Staten Island, Community District 3. The applicant also seeks an authorization pursuant to ZR Section 107-65 (“Modification of Existing Topography”), which is a ministerial action not subject to environmental review. The project site is situated in an R3-2 zoning district and is located within the Special South Richmond District (SSRD).

The project site, a 105,126 square foot lot, is developed with an approximately 50,000 gross square foot (gsf) Use Group 4 community facility building (the YMCA of Greater New York); a parking lot containing a mixture of both open and under-cover parking that totals 182 parking spaces, which are accessed via three existing curb cuts located on Richmond Avenue and Ridgcrest Avenue; and associated landscaping. An authorization to modify group parking facility regulations is required for any parking facility within the SSRD containing more than 30 parking spaces. Of the total 182 accessory parking spaces currently located on the project site, 107 spaces were subject to prior CPC actions (ULURP No. N940299RAR), approved in 1994. Since the 1994 CPC actions were approved, the YMCA facility has utilized a temporary parking area (containing 75 parking spaces) on an unpaved portion of lot 31. With the proposed authorization, the 75 space parking area would be paved and formalized and the YMCA facility would have a total capacity of 182 formal parking spaces. Vehicular access to the parking area on the project site would remain unchanged.

The proposed action would facilitate a proposal by the applicant to formalize a temporary, approximately 28,000 square foot (sf) area containing 75 parking spaces and associated parking lot landscaping accessory to an existing community facility building on the project site.

The analysis year for the proposed project is 2016.

Previous Approvals

In July 1981, CPC granted Land Use Application N 810061 RAR an authorization for modification of a group parking facility and access regulations pursuant to 107-68 to permit a parking area in excess of 30 parking spaces. An additional curb cut was authorized on Richmond Avenue pursuant to 107-68 as Richmond Avenue is designated a South Richmond Arterial Street pursuant to ZR Section 107-25 Special Regulations Along Certain Streets or Railroads.

In October 1994, the Board of Standards and Appeals (BSA) reviewed an application for the subject site, which was listed on BSA Calendar No. 7-94 BZ. The BSA adopted a resolution that facilitated construction of a two story and cellar community facility (Use Group 4) that exceeds height and setback requirements, which remains on the site and continues to operate as a community facility (YMCA).

In November 1994, the CPC approved Land Use Application N 940299 RAR, which granted authorizations for modification of existing topography pursuant to ZR 107-65, and modification of a group parking facility and access regulations pursuant to 107-68. A Commission Certification was also granted for substitution of other plant material pursuant to 107-323. The approval facilitated an expansion of the existing community facility (YMCA) by 39,486 sqft of floor area, and 124 formal parking spaces that would be accessed by two new 40 ft wide curb cuts from Richmond Avenue, and one new curb cut from Ridgecrest Avenue. The previously approved curb cut on Richmond Avenue (a designated arterial road) that had been approved by Land Use Application N 810061 RAR was proposed to be removed as part of this application.

B. DESCRIPTION OF THE PROPOSED PROJECT AREA/DEVELOPMENT SITE

As previously noted, Block 5236, Lot 31 is developed with an approximately 50,000 gsf community facility, a YMCA facility, 107 “formal” existing at-grade parking spaces and 75 temporary or informal parking spaces. The temporary parking area is located on a portion of the zoning lot on the south side of the property and occupies 28,050 sf of the project site. The project site is located on corner lot fronting three streets (Richmond Avenue on the west side; Oakdale Street on the south side; and Ridgecrest Avenue on the east side of the project site).

The temporary parking area was established for contractor vehicle staging and parking during renovation work at the existing building on the project site. Since the completion of that work, the temporary lot has been used informally as parking.

The project site is located on one zoning lot which has 105,126 sq. ft. of total lot area.

With regard to the zoning lot and street frontage widths, the west property frontage totals 446.82 ft. (80.00 ft. wide); the south property frontage line is 222.19 ft. (80.00 ft. wide); the east property frontage line is 541.61 ft. (60.00 ft. wide).

C. DESCRIPTION OF THE PROPOSED DEVELOPMENT

The applicant is seeking to formalize a temporary parking area of 75 new parking spaces bringing the overall total to 182 parking spaces within the project site. The area being formalized is located on the southeast portion of the project site.

There currently are three (3) 30 ft. curb cuts located on the project site. Two (2) are located on Richmond Avenue. One is dedicated to ingress only the other is dedicated to egress only. The third curb cut is located on Ridgecrest Avenue which is used for both ingress and egress. There are no new curb cuts proposed for this development.

II. BUILD YEAR

If approved, it is anticipated that the work will be completed in 2016.

III. PURPOSE AND NEED OF THE PROPOSED ACTIONS

The requested actions are needed to enable the applicant to formalize the temporary parking area which occupies 28,050 sf of the project site. Modification of more than 30 accessory off-street parking spaces within the Special South Richmond Zoning District requires Department of City Planning authorization.

IV. DEVELOPMENT SITES

There are no additional development sites.

V. NO ACTION SCENARIO

Absent the proposed action, the 75 informal parking spaces would be eliminated, and lot 31 would remain partially vacant.

VI. WITH ACTION SCENARIO

As previously described, in the future with the proposed action, the temporary parking area would be formalized adding a total of 75 parking spaces to the project site.



ATTACHMENT 'B':
SUPPLEMENTAL SCREENING

A. INTRODUCTION

This Environmental Assessment Statement (“EAS”) has been prepared in accordance with the guidelines and methodologies presented in the 2014 *City Environmental Quality Review (“CEQR”) Technical Manual*. For each technical area, thresholds are defined, which if met or exceeded, require that a detailed technical analysis be undertaken. Using these guidelines, preliminary screening assessments were conducted for the proposed action to determine whether detailed analysis of any technical area may be appropriate. Part II of the EAS Form identifies those technical areas that warrant additional assessment. For those technical areas that warranted a “Yes” answer in Part II of the EAS Form, including Land Use, Zoning, and Public Policy; Transportation and Air Quality supplemental screening assessments are provided in this attachment. The remaining technical areas detailed in the 2014 *CEQR Technical Manual* were not deemed to require supplemental screening because they do not trigger initial *CEQR* thresholds and/or are unlikely to result in significant adverse impacts.

The supplemental screening assessments contained herein identified that a preliminary assessment is required in the area of Land Use, Zoning, and Public Policy. That assessment is provided in Attachment ‘B’.

1. LAND USE, ZONING AND PUBLIC POLICY

The analysis of land use, zoning, and public policy characterizes the existing conditions of the project site and the surrounding study area; anticipates and evaluates those changes in land use, zoning, and public policy that are expected to occur independently of the proposed action; and identifies and addresses any potential impacts related to land use, zoning, and public policy resulting from the proposed project.

In order to assess the potential for project related impacts, the land use study area has been defined as the area located within a 400-foot radius of the project site, which is the area within which the proposed action has the potential to affect land use or land use trends. The 400-foot radius study area is generally bounded by Sycamore Avenue to the South, Thornycroft Avenue to the East, Amboy Road to the North and Retford Avenue to the West. Various sources have been used to prepare a comprehensive analysis of land use, zoning and public policy characteristics of the area, including field surveys, studies of the neighborhood and land use and zoning maps.

The action would facilitate the formalization of the temporary parking area.

The area in the vicinity of the project site is predominantly commercial and residential in nature and so the actions would not result in a change in land use or zoning that is different from the surrounding area. Nevertheless, the *CEQR Technical Manual* indicates that a preliminary assessment of zoning and land use is appropriate because this information, along with a general discussion of any applicable public policies, is useful for establishing a baseline for determining if detailed assessments are appropriate in other technical areas.

LAND USE

Land Use – Existing Conditions

The 105,126 gsf project site, located within Community Board 3, identified as Block 5236, Lot 31 in the Eltingville neighborhood of Staten Island, is developed with a 50,435 gsf community facility with 107 accessory parking spaces in the project area.

There is no change in land use or zoning being proposed under this application. The immediate surrounding area is a mix of detached and semi-detached one and two family homes, low rise row housing and low rise shopping centers. The majority of the buildings in the area are one or two story.

Surrounding Area

There are two contiguous lots, directly north of the project site both containing community facility uses, one is Regional Radiology the other is The YMCA Counseling Services. Further north along the Richmond Ave/Amboy Road intersection, there are a variety of low rise retail and office uses. Directly to the west of the project area along Richmond Avenue is low rise commercial

development. Further west a community facility and one and two story (one and two family) residences. To the south and east of the project site are one and two story (one and two family) residences.

There is a NYC MTA bus line that runs along Richmond Avenue with a stop directly in front on the project site and an MTA Staten Island Railway train stop two blocks northwest also on Richmond Avenue. Staten Island University Hospital South serves the area. Tottenville High School and IS 7 are located within 5 miles.

Land Use – Future With No Action (No-Build Conditions)

Absent the proposed actions, the predominantly commercial and residential areas land uses identified in the existing conditions would generally continue to exist and operate. Future land use development would be driven by market conditions that are generally favorable to additional retail and other commercial, residential and community facility uses surrounding the Richmond Avenue/Amboy Road corridors and adjacent areas, and institutional uses to support the existing residential areas.

Land Use - Future With Action (Build Conditions)

In the 2016 Build Conditions, the RWCDs for the parking lot formalization area would be the continued operation of the existing uses that consist of a YMCA facility and accessory on-site parking.

These uses are compatible with the existing predominantly commercial, community facility and residential uses along the Richmond Avenue/Amboy Road corridors, and therefore the proposed project would not result in significant impacts with respect to land use.

Land Use - Conclusion

The proposed formalization of the temporary parking area would be similar to and compatible with the existing commercial and community facility uses in the surrounding area. The project site is currently partially developed, and the proposed project would complement and strengthen the surrounding environment by providing additional formal parking spaces on the site.

No potentially significant adverse impacts related to land use are expected to occur as a result of the proposed action. Therefore, further analysis of land use is not warranted.

ZONING

Zoning – Existing Conditions

Zoning - Future Without the Action (No-Build Conditions)

In the future without the Proposed Actions, it is not expected that there will be any changes in the study areas existing zoning.

Zoning - Future With Action (Build Conditions)

As discussed above, in the future with the Proposed Actions, it is not expected that there will be any changes in the study areas existing zoning.

Conclusion

No significant impacts to zoning patterns in the area would be expected. The proposed project would comply with all the applicable requirements of the R3-2 zoning district and the Special South Richmond Development District (SSRDD) provisions of the Zoning Resolution. The proposed action would not have a significant impact on the extent of conformity with the current zoning in the surrounding area, and it would not adversely affect the viability of conforming uses on nearby properties.

Potentially significant adverse impacts related to zoning are not expected to occur as a result of the proposed action, and further assessment of zoning is not warranted.

PUBLIC POLICY

Public Policy – Existing Conditions

In addition to the public policies embodied in the existing zoning, public policy issues potentially affecting the Project Site include NYC's Waterfront Revitalization Program (WRP).

The New York City Waterfront Revitalization Program (WRP) is the city's principal coastal zone management tool. The WRP was originally adopted by the City of New York in 1982, revised in 2002, and was in the process of being updated in 2014. The WRP establishes the city's policies for development and use of the waterfront and provides the framework for evaluating the consistency of all discretionary actions in the coastal zone with those policies. The guiding principle of the WRP is to maximize the benefits derived from economic development, environmental preservation, and public use of the waterfront, while minimizing the conflicts among these objectives. Through individual project review, the WRP aims to promote activities appropriate to various waterfront locations. The program is designed to coordinate activities and decisions affecting the coast when there are overlapping jurisdictions or multiple discretionary actions. When a proposed project is located within the coastal zone and requires a local, state, or federal discretionary action, a determination of the project's consistency with the policies and intent of the WRP must be made before the project can move forward.

The Consistency Assessment Form (CAF) outlining the project description, purpose and location of activity, is part of this application and goes into greater explanation of how this project conforms to the applicable public policies and can be found under Appendix 'A' WRP Consistency Assessment Form.

The CAF was approved by the NYC Waterfront Revitalization program and was issued WRP #14-170.

Public Policy - Future Without the Action (No-Build Conditions)

There have not been any new programmed public policies identified that would affect the Project Site or the study area in the 2016 analysis year. The Project Site and the surrounding area would continue to be influenced by the policies currently in place and described in the Existing Conditions section above.

In the No-Action scenario the existing unoccupied area on the zoning lot would be vacant of any tenants or uses.

Public Policy - Future With Action (Build Conditions)

As noted above in the No-Action scenario section, there are no new public policies that have been identified that would affect the study area in the 2016 Build Year. Public policy in the future With Action scenario is expected to be driven by the same policies as currently in place, and that have been discussed above.

The Waterfront Consistency Assessment Form (CAF) and attached detailed assessments relating to the proposed project's consistency with the applicable waterfront policies is attached. The CAF and attached policy assessments conclude that the proposed project would be consistent with the policies embodied in the WRP, and further assessment is not warranted.

The proposed actions would facilitate a proposal by the applicant to formalize the temporary parking area.

As discussed, the proposed project is generally consistent with the policies embodied in NYC's Zoning Resolution and Waterfront Revitalization Program, and no further analysis is warranted.

Conclusion

The proposed activity is consistent with New York City's Waterfront Revitalization Program, pursuant to the New York State Coastal Management Program. Furthermore, the proposed activity complies with New York State's Coastal Management Program as expressed in New York City's approved Local Waterfront Revitalization Program, pursuant to New York State's Coastal Management Program, and will be conducted in a manner consistent with such program."

13. TRANSPORTATION

Transportation

The Applicant seeks a zoning authorization pursuant to Z.R. 107-68: Authorization of the Modification of Group Parking Facility and Access Regulations, to formalize a 75-space temporary parking lot at 3939 Richmond Avenue. Because the formalization of the temporary parking area exceeds the 60-space threshold size identified in 2014 CEQR Technical Manual (Table 19-1) for a parking facility in Zone 5, an analysis is required of the potential for the proposed Authorization to create significant adverse impacts related to transportation.

Project Description

The project site is 105,126 gsf in area developed with a 50,435 gsf community facility offering educational, health, and wellness programs for children, youth, and adults. It is located at 3939 Richmond Avenue, in the Eltingville section of Staten Island, one block south of Amboy Road and 1.5 blocks south of the Eltingville station of the Staten Island Rail Road. The temporary parking area is located at the southern portion of the project site. It is currently accessed via a gate leading to the existing parking area, and does not have direct access to public roads.

The temporary parking area was established for contractor vehicle staging and parking during renovation work at the existing building on the project site. Since the completion of that work, the temporary lot has been used informally as parking. The proposed Authorization would bring this use of the temporary parking area into compliance with the site's zoning. The formalized parking area would be striped for accessory parking use. No additional curb cuts would be provided. Access to the formalized parking area would continue to be solely from the project site's main parking lot, which itself has entrance and exit from Richmond Avenue. This analysis focuses on utilization of parking at the project site and its vicinity, and how changes to parking capacity as a result of the proposed action could affect traffic conditions in the area.

Existing Conditions

To document existing parking demand, counts of curbside parking utilization, and utilization of the project site's parking lot inclusive of the temporary parking area, were conducted on a typical midweek day, Thursday April 16, 2015. Curbside parking within one to two blocks of the site was surveyed, as shown in the attached figure. The parking study area is bounded by Sycamore Street one block south of the project site, Amboy Road one half block to the north, Retford Avenue two blocks to the west, and Thornycroft Avenue two blocks to the east. These counts were conducted during the project site's peak periods of activity as determined by member sign-in data. These

periods are 7 to 8 am and 6 to 7 pm. The parking availability is shown below, and detailed in the attached field data sheets.

<u>Survey Time</u>	<u>Available Curbside Spaces</u>	<u>Available On-Site Spaces</u>
7 AM	166	40
8 AM	212	24
6 PM	248	36
7 PM	236	23

Future No Action Condition

In a future condition without the proposed Authorization, the temporary parking lot would not be available for use by the project site patrons. Any parking demand that could not be met by the main 107-space existing parking lot would have to be met off-site. There are no off-street public parking facilities in the immediate project vicinity. Therefore nearby curbside parking would be utilized to accommodate any overflow from the project site's 107-space lot. Based on the parking data gathered in April 2015 as described above, the 107 space lot is inadequate to accommodate the project site's demand. Utilization of the existing lot, inclusive of the temporary parking area, exceeds the 107-space approved lot's capacity by 35, 51, 39, and 52 spaces during the 7 AM, 8 AM, 6 PM, and 7 PM periods. This excess demand would have to be accommodated at curbside locations within the project vicinity.

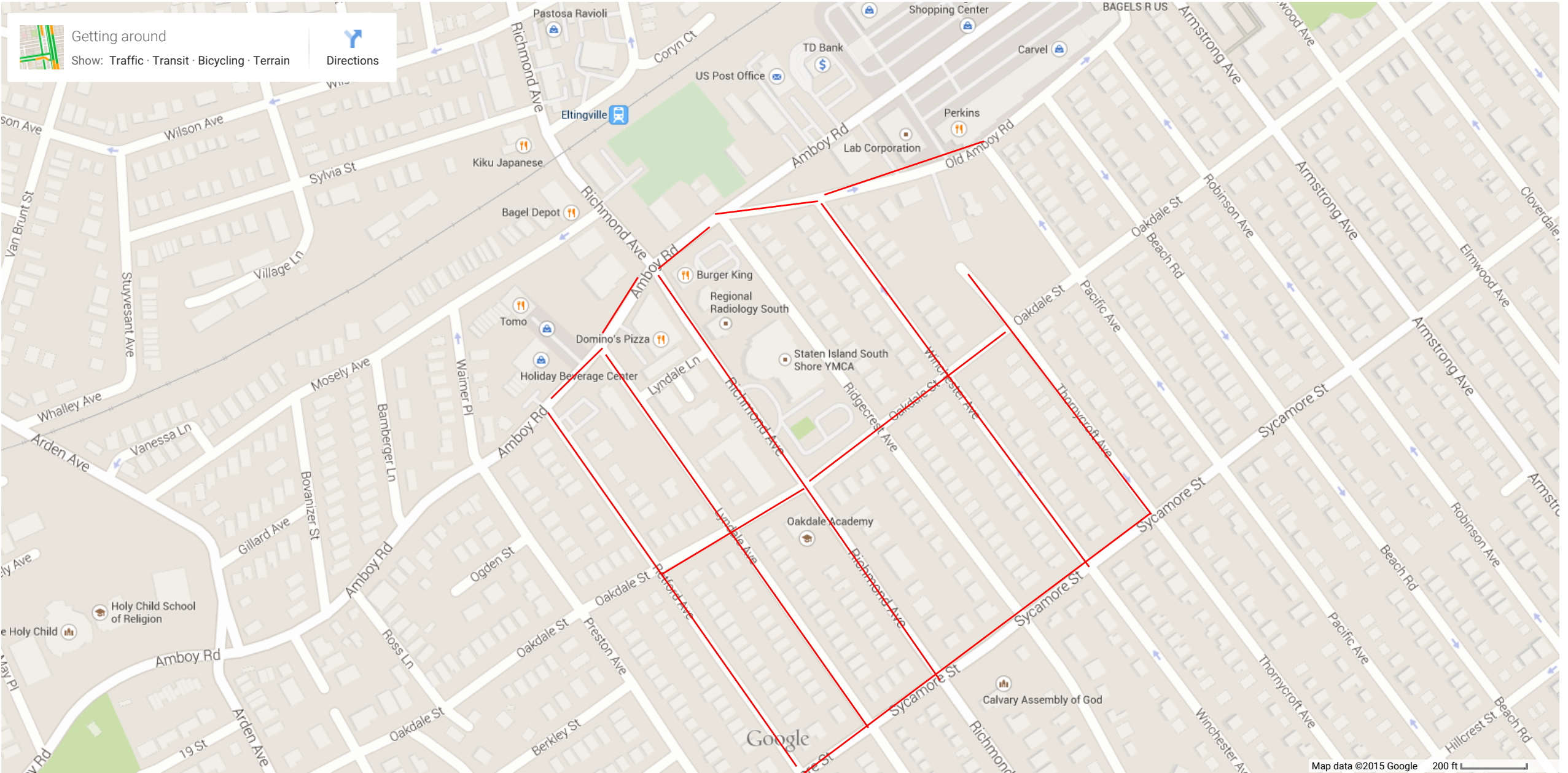
As documented in the curbside parking survey conducted during the same periods as the parking lot counts, there are over 150 available curbside parking spaces within the parking survey area during all analysis times. The project site's parking demand in excess of the 107 spaces currently approved for the site would be met by available curbside spaces. Curbside parking is widely available on the blocks immediately surrounding the project site and is adequate to accommodate excess demand.

Future With Action Condition

In the future with the proposed Authorization, the 75-space temporary parking area would continue to be used as accessory parking for the project site patrons. Therefore future with action condition would be a continuation of existing conditions, with 182 accessory parking spaces available for accessory parking on the project site. As documented during the parking survey conducted in April 2015, occupancy of the parking facility during peak hours of project site activity ranged between 78% and 87% of capacity.

Conclusion

The proposed action would formalize the 75-space temporary parking area located at the project site. As compared to a no-action condition, up to 51 vehicles that would require curbside parking in a no-action condition would instead park at the site. In a no-action condition these 51 vehicles would be dispersed to curbside locations on block fronts in the surrounding area, while under the with-action condition they would terminate and originate at the project site. Because curbside parking is dispersed to the north, south, east, and west of the project site, this reassignment of trips associated with the project site from curbside locations to the project site itself would not result in incremental traffic beyond fifty vehicles during any hour at any individual intersection. To the extent that the provision of additional parking at the site would eliminate the need for project site patrons to travel to the site and, upon finding no parking availability, then return to area streets to find a curbside space, the proposed action would reduce traffic in the surrounding area. Therefore, pursuant to the methodology of the 2014 *CEQR Technical Manual*, the proposed action does not have the potential to result in significant adverse impacts related to transportation, and no further analysis is warranted.



ON-STREETS PARKING SURVEY

Project: "Staten Island YMCA"

Day/Date: Thursday 4/16/15

Surveyors: _____

Av./St. Side	<u>Retford Ave</u> West b-n Sycamore St		<u>Retford Ave</u> East b-n Sycamore St		<u>Retford Ave</u> West b-n Oakdale St		<u>Retford Ave</u> East b-n Oakdale St		<u>Lyndale Ave</u> West b-n Sycamore St		<u>Lyndale Ave</u> East b-n Sycamore St		<u>Lyndale Ave</u> West b-n Oakdale St		<u>Lyndale Ave</u> East b-n Oakdale St	
	Oakdale St		Oakdale St		Amboy Rd		Amboy Rd		Oakdale St		Oakdale St		Amboy Rd		Amboy Rd	
Start Time	Parked	Empty	Parked	Empty	Parked	Empty	Parked	Empty	Parked	Empty	Parked	Empty	Parked	Empty	Parked	Empty
7:00 AM	10	10	9	7	11	4	6	8	17	3	13	4	8	2	14	1
8:00 AM	8	12	7	9	9	6	5	9	14	6	12	5	9	1	15	0
6:00 PM	10	10	7	9	9	6	6	8	15	5	14	3	4	6	7	8
7:00 PM	9	11	8	8	9	6	8	6	15	5	15	2	3	7	11	4

Av./St. Side	<u>Richmond Ave</u> West b-n Sycamore St		<u>Richmond Ave</u> East b-n Sycamore St		<u>Richmond Ave</u> West b-n Oakdale St		<u>Richmond Ave</u> East b-n Oakdale St		<u>Ridgecrest Ave</u> West b-n Sycamore St		<u>Ridgecrest Ave</u> East b-n Sycamore St		<u>Ridgecrest Ave</u> West b-n Oakdale St		<u>Ridgecrest Ave</u> East b-n Oakdale St	
	Oakdale St		Oakdale St		Amboy Rd		Amboy Rd		Oakdale St		Oakdale St		Amboy Rd		Amboy Rd	
Start Time	Parked	Empty	Parked	Empty	Parked	Empty	Parked	Empty	Parked	Empty	Parked	Empty	Parked	Empty	Parked	Empty
7:00 AM	12	4	9	4	12	2	10	1	14	1	14	4	26	1	18	3
8:00 AM	10	6	7	6	9	5	11	0	15	0	13	5	25	2	17	4
6:00 PM	13	3	12	1	14	0	7	4	8	7	11	7	27	0	17	4
7:00 PM	13	3	9	4	12	2	10	1	13	2	10	8	24	3	17	4

Av./St. Side	<u>Winchester Ave</u> West b-n Sycamore St		<u>Winchester Ave</u> East b-n Sycamore St		<u>Winchester Ave</u> West b-n Oakdale St		<u>Winchester Ave</u> East b-n Oakdale St		<u>Thornycroft Ave</u> West b-n Sycamore St		<u>Thornycroft Ave</u> East b-n Sycamore St		<u>Thornycroft Ave</u> West b-n Oakdale St		<u>Thornycroft Ave</u> East b-n Oakdale St	
	Oakdale St		Oakdale St		Old Amboy Rd		Old Amboy Rd		Oakdale St		Oakdale St		Dead End		Dead End	
Start Time	Parked	Empty	Parked	Empty	Parked	Empty	Parked	Empty	Parked	Empty	Parked	Empty	Parked	Empty	Parked	Empty
7:00 AM	11	5	11	5	14	3	13	5	11	5	14	4	0	5	0	5
8:00 AM	9	7	12	4	11	6	10	8	9	7	11	7	0	5	0	5
6:00 PM	10	6	9	7	15	2	16	2	10	6	13	5	0	5	0	5
7:00 PM	12	4	12	4	15	2	16	2	11	5	13	5	0	5	0	5

ON-STREETS PARKING SURVEY

Project: "Staten Island YMCA"

Day/Date: Thursday 4/16/15

Surveyors: _____

Av/St. Side	Amboy		Amboy		Old Amboy		Old Amboy		Old Amboy		YMCA Parking	Available Spaces - 182 Space Lot	Parking Shortfall - 107 Space Lot	Percentage of 182- space lot capacity
	North Richmond Ave Ridgecrest Ave	b-n	South Richmond Ave Ridgecrest Ave	b-n	North Ridgecrest Ave Pacific St	b-n	South Ridgecrest Ave Winchester Ave	b-n	South Winchester Ave Pacific St	b-n				
Start Time	Parked	Empty	Parked	Empty	Parked	Empty	Parked	Empty	Parked	Empty	Parked			
7:00 AM	0	0	0	0	7	8	7	1	6	2	142	40	35	78%
8:00 AM	0	0	0	0	8	7	8	0	5	3	158	24	51	87%
6:00 PM	0	0	0	0	2	13	1	7	1	7	146	36	39	80%
7:00 PM	0	0	0	0	0	15	3	5	4	4	159	23	52	87%

Study Area Total

Start Time	Parked	Empty
7:00 AM	391	166
8:00 AM	345	212
6:00 PM	309	248
7:00 PM	317	236

14. AIR QUALITY

Under CEQR, two potential types of air quality effects are examined. These are mobile and stationary source impacts. Potential mobile source impacts are those which could result from an increase in traffic in the area, resulting in greater congestion and higher levels of carbon monoxide (CO). Potential stationary source impacts are those that could occur from stationary sources of air pollution, such as major industrial processes or heat and hot water boilers of major buildings in close proximity to a proposed project. Both the potential impacts of a proposed project on surrounding buildings and potential impacts of uses in the environs of a proposed sensitive use, such as residences, schools, and hospitals, are considered in the assessment.

Mobile Source

Under guidelines contained in the *CEQR Technical Manual*, and in this area of New York City, projects generating fewer than 170 additional vehicular trips in any given hour are considered as highly unlikely to result in significant mobile source impacts, and do not warrant detailed mobile source air quality studies. The proposed development would generate fewer than 170 vehicle trips at any intersection in the study area during any peak hour. Additionally, it is not projected to generate peak hour heavy-duty diesel vehicular traffic above the *CEQR Technical Manual*, January 2014 Edition threshold of 12 HDDV vehicles. Therefore, no detailed mobile source air quality analysis would be required per the *CEQR Technical Manual*, and no significant mobile source air quality impacts would be generated by proposed action.

The proposed development would formalize 75 temporary accessory parking spaces, which represents the net increment between the no action and with action scenarios. Per the guidelines of the *CEQR Technical Manual*, an air quality analysis for this 75-space parking lot is not warranted.

Stationary Source

The proposed action would not be expected to have any effect on stationary source air emissions, and no further analysis is warranted.

APPENDIX 'A'
WRP CONSISTENCY ASSESSMENT FORM

For Internal Use Only:

Date Received: _____

WRP no. _____

DOS no. _____

NEW YORK CITY WATERFRONT REVITALIZATION PROGRAM Consistency Assessment Form

Proposed actions that are subject to CEQR, ULURP or other local, state or federal discretionary review procedures, and that are within New York City's designated coastal zone, must be reviewed and assessed for their consistency with the New York City Waterfront Revitalization Program (WRP). The WRP was adopted as a 197-a Plan by the Council of the City of New York on October 13, 1999, and subsequently approved by the New York State Department of State with the concurrence of the United States Department of Commerce pursuant to applicable state and federal law, including the Waterfront Revitalization of Coastal Areas and Inland Waterways Act. As a result of these approvals, state and federal discretionary actions within the city's coastal zone must be consistent to the maximum extent practicable with the WRP policies and the city must be given the opportunity to comment on all state and federal projects within its coastal zone.

This form is intended to assist an applicant in certifying that the proposed activity is consistent with the WRP. It should be completed when the local, state, or federal application is prepared. The completed form and accompanying information will be used by the New York State Department of State, other state agencies or the New York City Department of City Planning in their review of the applicant's certification of consistency.

A. APPLICANT

1. Name: James Heineman, Equity Environmental Engineering LLC
2. Address: 227 Route 206 Suite 6, Flanders NJ 07836
3. Telephone: 973-527-7451 Fax: 973-858-0280 E-mail: jim.heineman@equityenvironmental.com
4. Project site owner: YMCA of Greater New York

B. PROPOSED ACTIVITY

1. Brief description of activity:
The South Shore YMCA, at 3939 Richmond Road, Staten Island NY, seeks a zoning authorization pursuant to Zoning Resolution Section 107-68: Authorization of the Modification of Group Parking Facility and Access Regulations, to allow the formalization of the temporary parking area at the referenced project site.
2. Purpose of activity:
The proposed authorization would allow continued use of the parking lot enlargement by patrons of the YMCA's educational, health, and wellness programs for children, youth, and adults.
3. Location of activity: (street address/borough or site description):
3939 Richmond Road, Staten Island

Proposed Activity Cont'd

- 4. If a federal or state permit or license was issued or is required for the proposed activity, identify the permit type(s), the authorizing agency and provide the application or permit number(s), if known:
none

- 5. Is federal or state funding being used to finance the project? If so, please identify the funding source(s).
no

- 6. Will the proposed project require the preparation of an environmental impact statement?
Yes _____ No ✓ If yes, identify Lead Agency:

- 7. Identify **city** discretionary actions, such as a zoning amendment or adoption of an urban renewal plan, required for the proposed project.
Zoning Authorization pursuant to Zoning Resolution section 107-68:
Authorization of the Modification of Group Parking Facility and Access Regulations

C. COASTAL ASSESSMENT

Location Questions:

Yes No

- | | | |
|---|-------|---|
| 1. Is the project site on the waterfront or at the water's edge? | _____ | ✓ |
| 2. Does the proposed project require a waterfront site? | _____ | ✓ |
| 3. Would the action result in a physical alteration to a waterfront site, including land along the shoreline, land underwater, or coastal waters? | _____ | ✓ |

Policy Questions

Yes No

The following questions represent, in a broad sense, the policies of the WRP. Numbers in parentheses after each question indicate the policy or policies addressed by the question. The new Waterfront Revitalization Program offers detailed explanations of the policies, including criteria for consistency determinations.

Check either "Yes" or "No" for each of the following questions. For all "yes" responses, provide an attachment assessing the effects of the proposed activity on the relevant policies or standards. Explain how the action would be consistent with the goals of those policies and standards.

- | | | |
|---|-------|---|
| 4. Will the proposed project result in revitalization or redevelopment of a deteriorated or under-used waterfront site? (1) | _____ | ✓ |
| 5. Is the project site appropriate for residential or commercial redevelopment? (1.1) | _____ | ✓ |
| 6. Will the action result in a change in scale or character of a neighborhood? (1.2) | _____ | ✓ |

Policy Questions cont'd

Yes No

7. Will the proposed activity require provision of new public services or infrastructure in undeveloped or sparsely populated sections of the coastal area? (1.3)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
8. Is the action located in one of the designated Significant Maritime and Industrial Areas (SMIA): South Bronx, Newtown Creek, Brooklyn Navy Yard, Red Hook, Sunset Park, or Staten Island? (2)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
9. Are there any waterfront structures, such as piers, docks, bulkheads or wharves, located on the project sites? (2)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
10. Would the action involve the siting or construction of a facility essential to the generation or transmission of energy, or a natural gas facility, or would it develop new energy resources? (2.1)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
11. Does the action involve the siting of a working waterfront use outside of a SMIA? (2.2)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
12. Does the proposed project involve infrastructure improvement, such as construction or repair of piers, docks, or bulkheads? (2.3, 3.2)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
13. Would the action involve mining, dredging, or dredge disposal, or placement of dredged or fill materials in coastal waters? (2.3, 3.1, 4, 5.3, 6.3)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
14. Would the action be located in a commercial or recreational boating center, such as City Island, Sheepshead Bay or Great Kills or an area devoted to water-dependent transportation? (3)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
15. Would the proposed project have an adverse effect upon the land or water uses within a commercial or recreation boating center or water-dependent transportation center? (3.1)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
16. Would the proposed project create any conflicts between commercial and recreational boating? (3.2)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
17. Does the proposed project involve any boating activity that would have an impact on the aquatic environment or surrounding land and water uses? (3.3)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
18. Is the action located in one of the designated Special Natural Waterfront Areas (SNWA): Long Island Sound- East River, Jamaica Bay, or Northwest Staten Island? (4 and 9.2)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
19. Is the project site in or adjacent to a Significant Coastal Fish and Wildlife Habitat? (4.1)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
20. Is the site located within or adjacent to a Recognized Ecological Complex: South Shore of Staten Island or Riverdale Natural Area District? (4.1and 9.2)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
21. Would the action involve any activity in or near a tidal or freshwater wetland? (4.2)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
22. Does the project site contain a rare ecological community or would the proposed project affect a vulnerable plant, fish, or wildlife species? (4.3)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
23. Would the action have any effects on commercial or recreational use of fish resources? (4.4)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
24. Would the proposed project in any way affect the water quality classification of nearby waters or be unable to be consistent with that classification? (5)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
25. Would the action result in any direct or indirect discharges, including toxins, hazardous substances, or other pollutants, effluent, or waste, into any waterbody? (5.1)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
26. Would the action result in the draining of stormwater runoff or sewer overflows into coastal waters? (5.1)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
27. Will any activity associated with the project generate nonpoint source pollution? (5.2)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
28. Would the action cause violations of the National or State air quality standards? (5.2)	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Policy Questions cont'd

Yes No

29. Would the action result in significant amounts of acid rain precursors (nitrates and sulfates)? (5.2C)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
30. Will the project involve the excavation or placing of fill in or near navigable waters, marshes, estuaries, tidal marshes or other wetlands? (5.3)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
31. Would the proposed action have any effects on surface or ground water supplies? (5.4)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
32. Would the action result in any activities within a federally designated flood hazard area or state-designated erosion hazards area? (6)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
33. Would the action result in any construction activities that would lead to erosion? (6)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
34. Would the action involve construction or reconstruction of a flood or erosion control structure? (6.1)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
35. Would the action involve any new or increased activity on or near any beach, dune, barrier island, or bluff? (6.1)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
36. Does the proposed project involve use of public funds for flood prevention or erosion control? (6.2)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
37. Would the proposed project affect a non-renewable source of sand ? (6.3)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
38. Would the action result in shipping, handling, or storing of solid wastes, hazardous materials, or other pollutants? (7)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
39. Would the action affect any sites that have been used as landfills? (7.1)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
40. Would the action result in development of a site that may contain contamination or that has a history of underground fuel tanks, oil spills, or other form or petroleum product use or storage? (7.2)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
41. Will the proposed activity result in any transport, storage, treatment, or disposal of solid wastes or hazardous materials, or the siting of a solid or hazardous waste facility? (7.3)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
42. Would the action result in a reduction of existing or required access to or along coastal waters, public access areas, or public parks or open spaces? (8)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
43. Will the proposed project affect or be located in, on, or adjacent to any federal, state, or city park or other land in public ownership protected for open space preservation? (8)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
44. Would the action result in the provision of open space without provision for its maintenance? (8.1)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
45. Would the action result in any development along the shoreline but NOT include new water-enhanced or water-dependent recreational space? (8.2)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
46. Will the proposed project impede visual access to coastal lands, waters and open space? (8.3)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
47. Does the proposed project involve publicly owned or acquired land that could accommodate waterfront open space or recreation? (8.4)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
48. Does the project site involve lands or waters held in public trust by the state or city? (8.5)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
49. Would the action affect natural or built resources that contribute to the scenic quality of a coastal area? (9)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
50. Does the site currently include elements that degrade the area's scenic quality or block views to the water? (9.1)	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Policy Questions cont'd

Yes No

51. Would the proposed action have a significant adverse impact on historic, archeological, or cultural resources? (10)

52. Will the proposed activity affect or be located in, on, or adjacent to an historic resource listed on the National or State Register of Historic Places, or designated as a landmark by the City of New York? (10)

D. CERTIFICATION

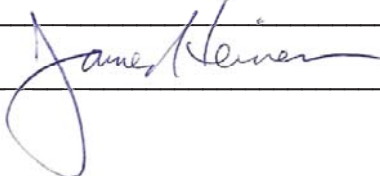
The applicant or agent must certify that the proposed activity is consistent with New York City's Waterfront Revitalization Program, pursuant to the New York State Coastal Management Program. If this certification cannot be made, the proposed activity shall not be undertaken. If the certification can be made, complete this section.

"The proposed activity complies with New York State's Coastal Management Program as expressed in New York City's approved Local Waterfront Revitalization Program, pursuant to New York State's Coastal Management Program, and will be conducted in a manner consistent with such program."

Applicant/Agent Name: James Heineman, Equity Environmental Engineering LLC

Address: 227 Route 206, Suite 6

Flanders NJ 07836 Telephone 973-527-7451

Applicant/Agent Signature:  Date: June 2, 2015