

CB#6 197a Plan

197a Plan
For
The Eastern Section of
Community District 6

Borough of Manhattan
New York City

Prepared with the assistance of:

Buckhurst Fish & Jacquemart

CB#6: 14 Street on the south
59 Street on the north
Lexington and Madison
Avenues on the west
East River on the east.

197a: 14 Street on the south
59 Street on the north
Second Avenue plus 100
feet on the west
East River on the east.

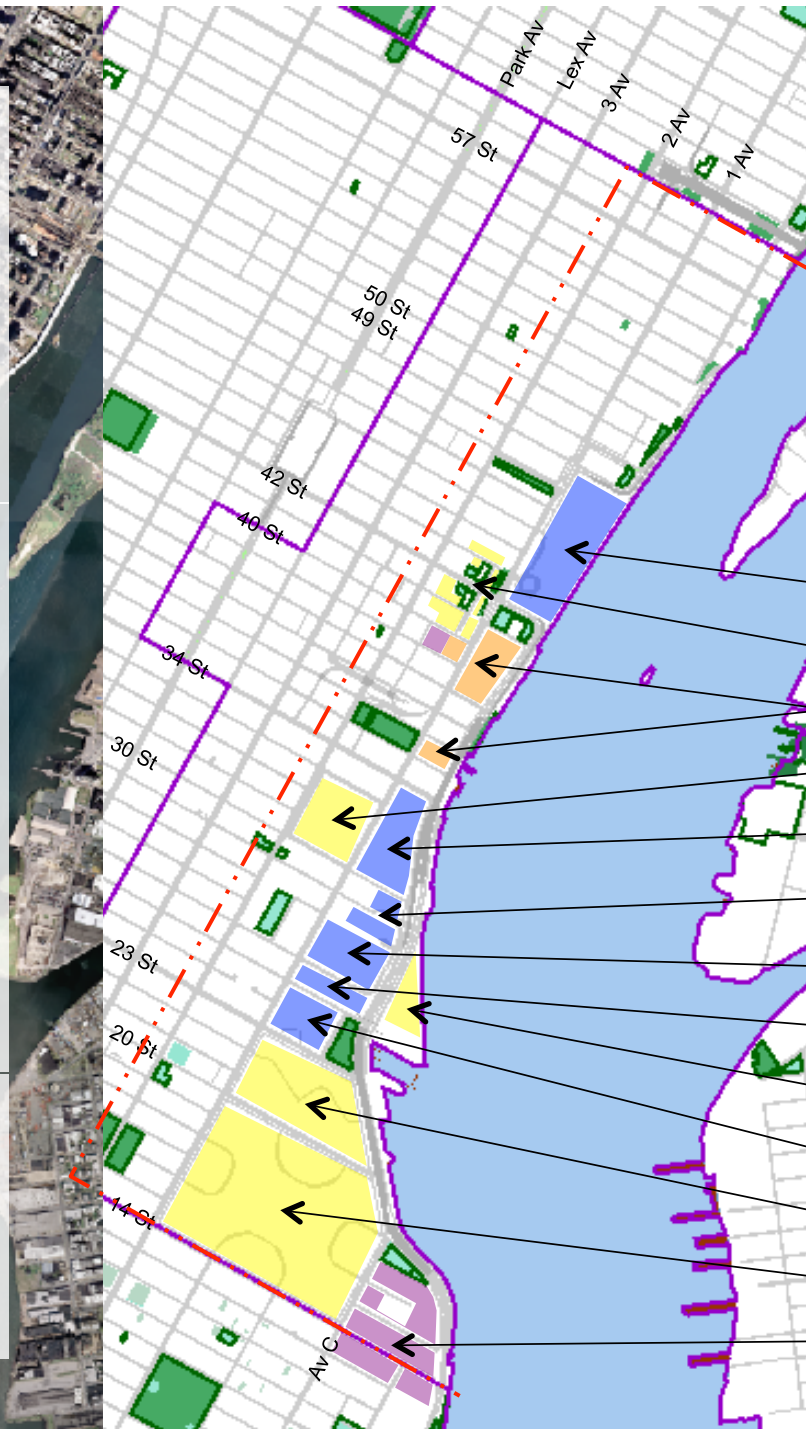
Community Board Six's 197a plan has four themes:

Land Use: This is a community of residential neighborhoods cohabiting with institutional uses -- hospitals and the United Nations. CD#6 is near the midtown central district but, except for a small area near Grand Central and along Third Avenue, it is separate

Waterfront and Open Space: CD#6 is the community district with the least open space per person in the city. The waterfront is a last opportunity. The goal is a continuous esplanade along the river and access at as many streets as possible.

Urban Design and Preservation: The goal is investment that makes the best use of the existing built fabric of the community and adds new buildings and infrastructure that respect that context.

Transportation: CD#6 is pedestrian and transit friendly. The goal is to make it more so by restoring missing streets to the grid, welcoming the Second Avenue subway, and minimizing parking and traffic.

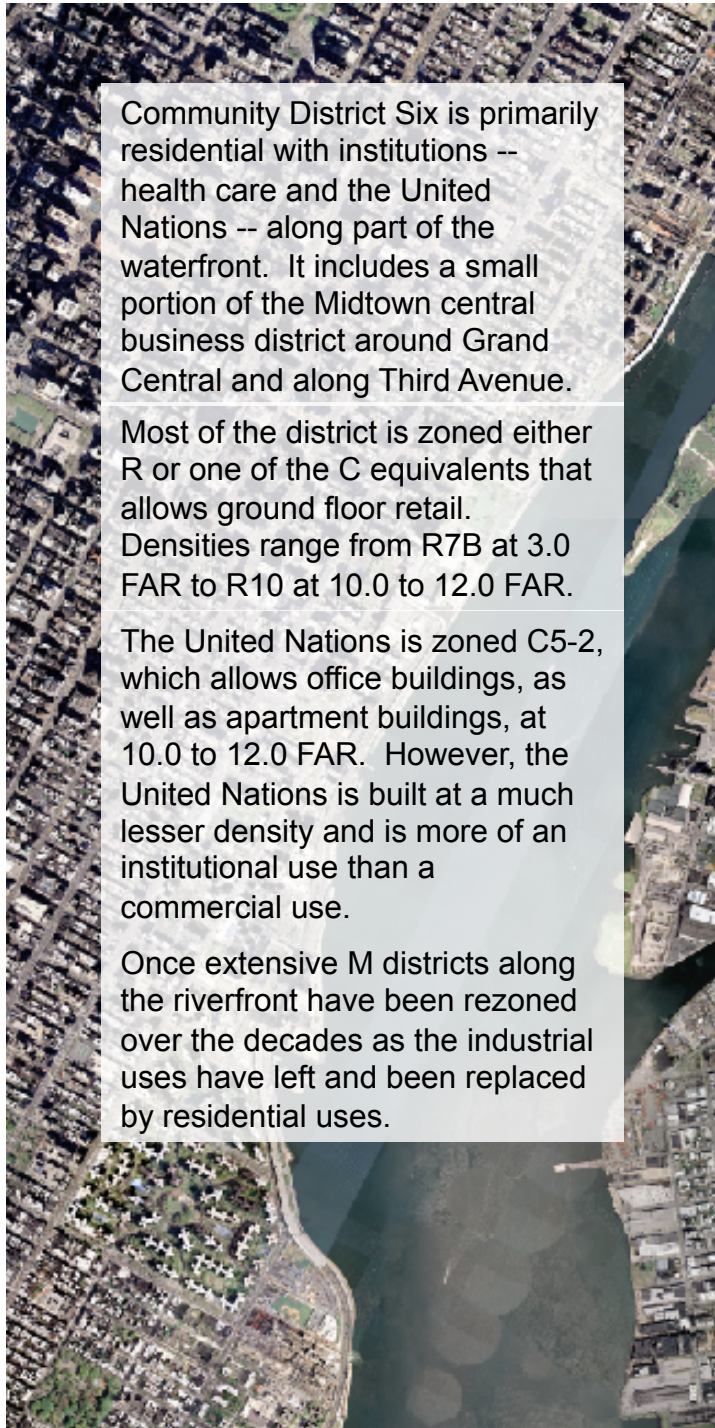


CB#6 197a Plan Orientation

CB#6: 14 Street on the south
59 Street on the north
Lexington and Madison
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United Nations.
Tudor City.
Former Con Ed Waterside.
Kips Bay.
NYU Medical Center.
East River Science Park.
Bellevue Hospital.
Brookdale Center.
Waterside Plaza.
Veterans Administration Hospital.
Peter Cooper Village.
Stuyvesant Town.
Con Ed East River.

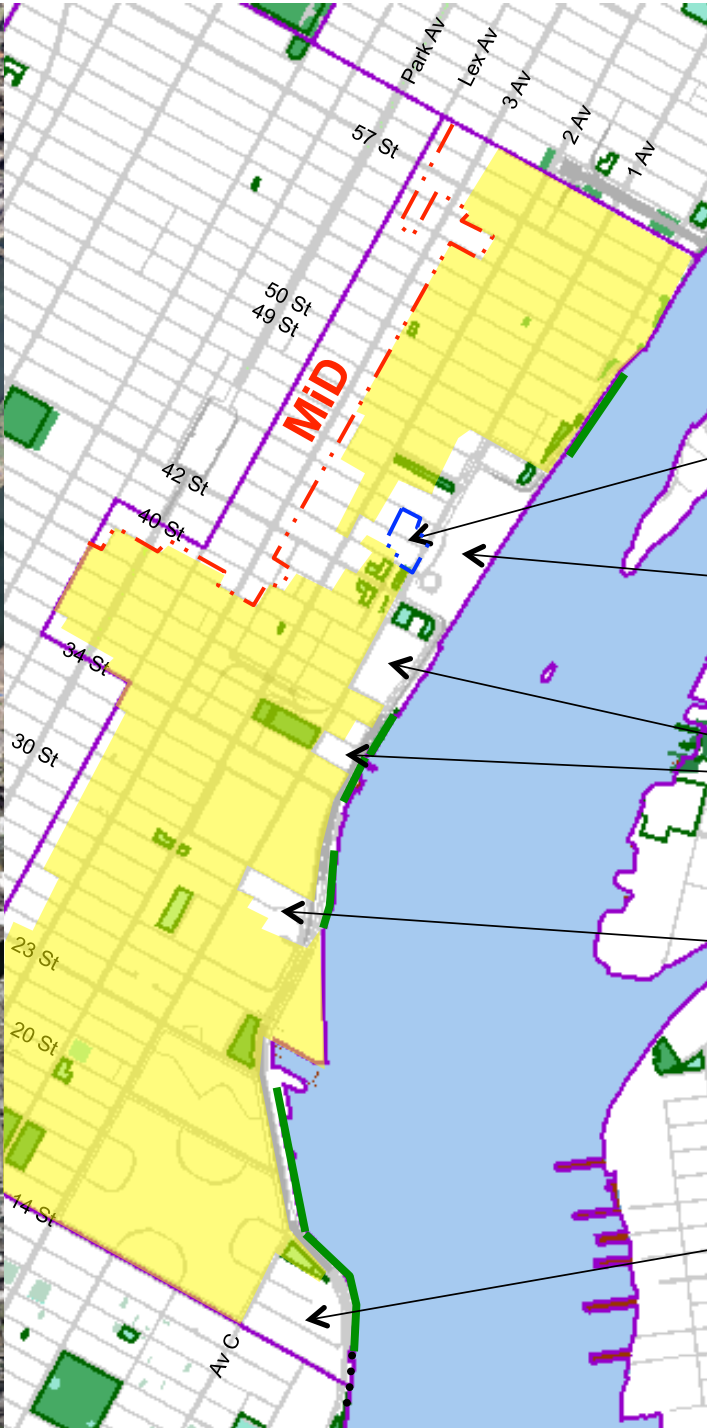


Community District Six is primarily residential with institutions -- health care and the United Nations -- along part of the waterfront. It includes a small portion of the Midtown central business district around Grand Central and along Third Avenue.

Most of the district is zoned either R or one of the C equivalents that allows ground floor retail. Densities range from R7B at 3.0 FAR to R10 at 10.0 to 12.0 FAR.

The United Nations is zoned C5-2, which allows office buildings, as well as apartment buildings, at 10.0 to 12.0 FAR. However, the United Nations is built at a much lesser density and is more of an institutional use than a commercial use.

Once extensive M districts along the riverfront have been rezoned over the decades as the industrial uses have left and been replaced by residential uses.



CB#6 197a Plan

Land Use

Most of CD#6 is residential, shown in yellow -- R7-2, R7B, R8, R8A, R8B, R10, C1-8, C1-8A, C1-9, C1-9A, and C2-7.

The Special Midtown District -- MiD -- is outlined in red.

Special United Nations Development District -- U -- outlined in blue.

United Nations -- C5-2.

Former Con Ed Waterside -- C5-2 and C4-6.

East River Science Park -- C6-2.

Con Ed East River -- M3-2.

CB#6 197a Plan

Waterfront

Only seven streets provide pedestrian access to the waterfront.

Only 18 blocks have a public walkway at the water's edge.

Former outer detour roadway.

51 - 54 Streets.

49 - 51 Streets.

United Nations -- 42-49 Streets.

41 - 42 Streets.

Former Con Ed pier -- 38-41 Streets.

"Glick" Park -- 36-38 Streets.

Ferry pier -- 34-36 Streets.

Heliport -- 32-34 Streets.

Water Club -- 29-32 Streets.

Waterside -- 25-29 Streets.

UN School -- 24-25 Streets.

23 - 24 Streets.

Stuyvesant Cove -- 18-23 Streets.

15 - 18 Streets.

Con Ed East River.



JPW III
28 Nov 07

Currently 18 of the 45 blocks of waterfront between 14 and 59 Streets have a public walkway along the edge of the river.

At the south there is a connection to East River Park but the walkway is only a few feet wide.

Between 23 and 29 Streets the path follows the service road of the FDR Drive. The walkway along the water at Waterside is gated closed and the water's edge at the UN School is used for parking.

Between 32 and 34 Streets the path is inboard of the heliport, along the FDR Drive service road.

Between 38 and 42 Streets there is an opportunity to reuse the former Con Ed pier, which belongs to the City, and add a ramp to the foot of 42 Street.

The esplanade above the FDR Drive at the UN is not available to the public but a walkway has been proposed above the water between 42 and 51 Streets.

North of 54 Street it may be possible to continue the esplanade on part of the former outer detour roadway.



Community Board Six has recommended a **waterfront access district** similar to that established across the river in Greenpoint Williamsburg. However, such a district does not apply in CD#6 because the first upland street is the FDR Drive.

Subsequently, CB#6 proposed a **special district** for the area between 34 and 41 Streets to accomplish the goals of the waterfront access district.

CB#6 has for a long time recommended a **Special Hospital Use District** for the area between 23 and 34 Streets to protect the hospitals from the intrusion of incompatible uses.

During the summer and fall of 2001 CB#6 agreed on a site plan for the **East River Science Park**.

CB#6 has for a long time recommended that Stuyvesant Town and Peter Cooper Village be mapped as **Special Planned Community Preservation Districts**.

CB#6 197a Plan Urban Design

Much of CD#6 has been rezoned with contextual districts. They and the tower-on-a-base provisions in other districts go far to mold new buildings to the built neighborhood context.

Recommended Special Public Access District outlined in blue.

United Nations.

Robert Moses Playground -- proposed site for expansion of United Nations.

Proposed Special East River Access District -- C 060394 ZRM -- shown in yellow.

Agreed site plan for the East River Science Park.

Recommended Special Hospital Use District.

Recommended Special Planned Community Preservation District.

Recommended site plan for Con Ed East River to consolidate the ball fields with Murphy Brothers Playground and reopen 14 Street with southbound egress and access for the FDR Drive.

Transportation in Community District Six is based on pedestrians and transit, not on automobiles.

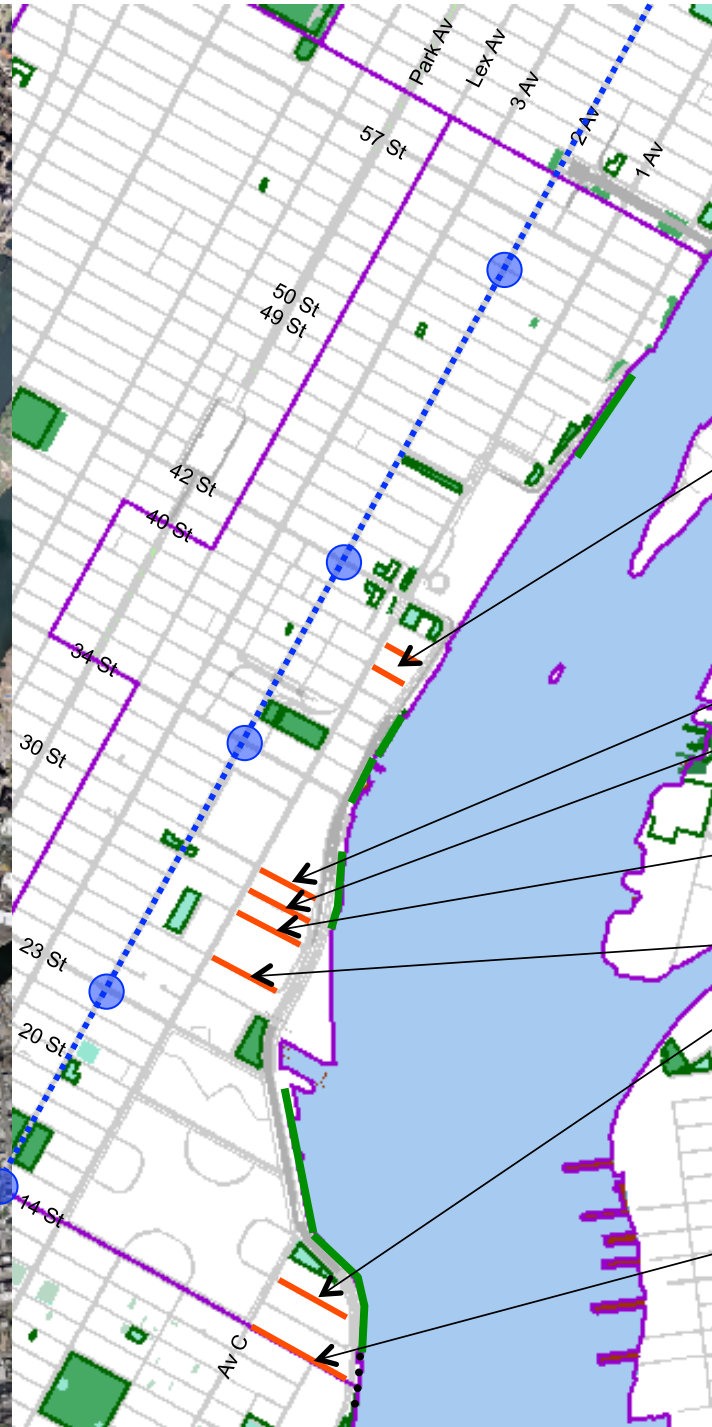
CD#6 is of a walkable scale for local shopping, services, and entertainment. It is within walking distance of parts of Midtown. It has excellent access by transit to other parts of the city. And in a pinch one can sometimes catch a cab.

The goal is to build on these advantages by restoring missing streets, encouraging construction of the Second Avenue subway, and limiting the amount of parking.

The **missing streets** are generally in the blocks adjacent to the waterfront. Restoring them improves access to buildings in those blocks and also improves access to the waterfront.

The community has been waiting for the **Second Avenue subway** since the elevated was demolished.

Less parking -- say 10% accessory rather than 20% -- would reduce traffic and improve air quality.



CB#6 197a Plan Transportation

The Second Avenue subway will provide greatly improved transit access for CD#6.

Restore missing streets and improve access to the waterfront:

Remap 39 and 40 Streets and rebuild them high enough at their east ends to allow for pedestrian bridges to the waterfront. They were closed for security during World War II and it is time for them to be restored to public use.

Remap existing 30 Street.

29 Street being rebuilt to be high enough at east end for a pedestrian bridge to the waterfront.

28 Street being opened for ambulance access to Bellevue.

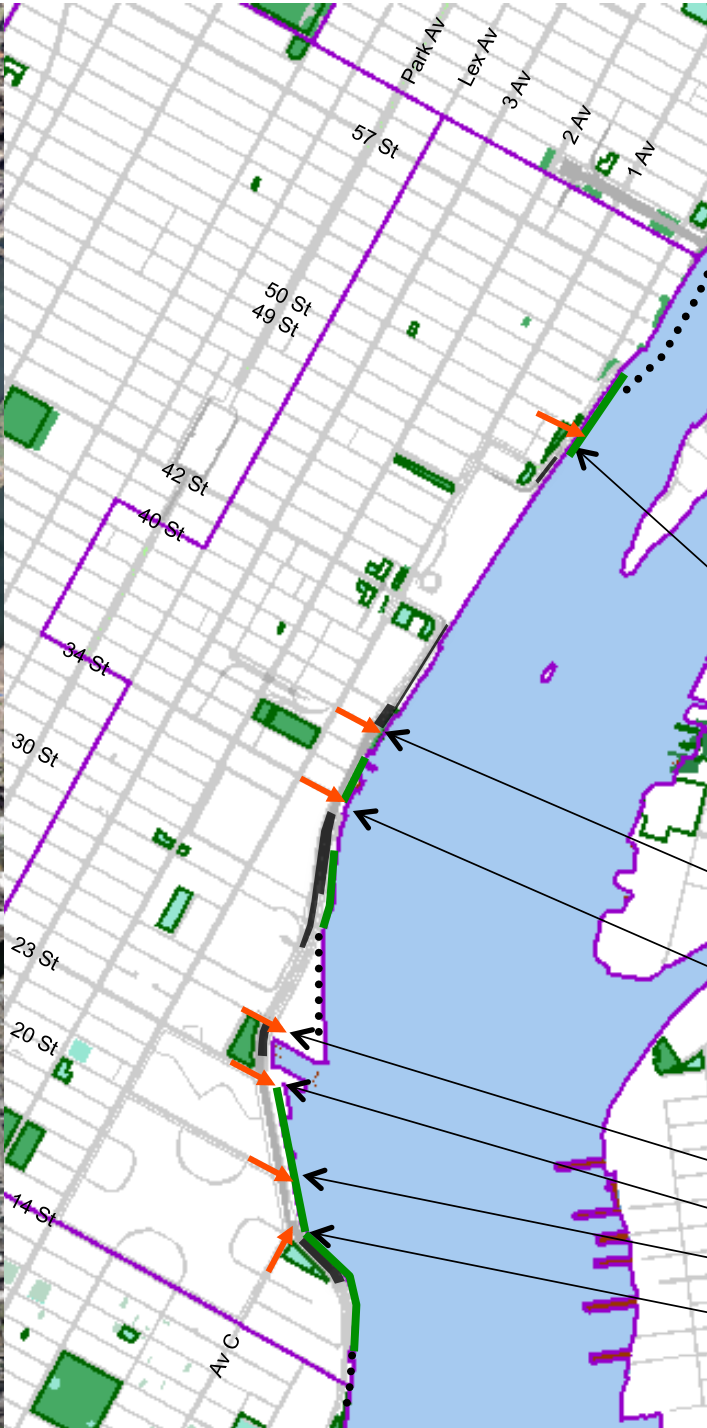
Remap existing 26 Street.

Extend 16 Street to provide pedestrian access to the east river esplanade. Consider trading for 15 Street in order to consolidate Con Ed Riverside and enlarge Murphy Brothers Playground by relocating the Con Ed ball fields.

Reopen 14 Street, which was closed for security after 9-11, and provide southbound egress and access to the FDR Drive.



JPW III
28 Nov 07



CB#6 197a Plan

Waterfront Access

Only seven streets currently provide pedestrian access to the waterfront.

Access has been planned or discussed at ten other streets and is possible at several more locations.

- 59 to 60 Streets
- 55 to 58 Streets
- 53 to 54 Streets
- 52 Street
- 51 Street**
- 49 to 50 Streets
- 48 Street
- 41 to 47 Streets
- 42 Street
- 38 to 41 Streets
- 37 Street**
- 35 and 36 Streets
- 34 Street**
- 30 to 32 Streets
- 29 Street
- 27 Street
- 25 Street**
- 23 Street**
- 20 Street**
- Avenue C**
- 16 Street

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CB#6 Waterfront 15 to 18 Streets

There is no access to the esplanade between 18 Street and 10 Street. A pedestrian bridge at 16 Street would improve access and align with the circulation system of Stuyvesant Town.

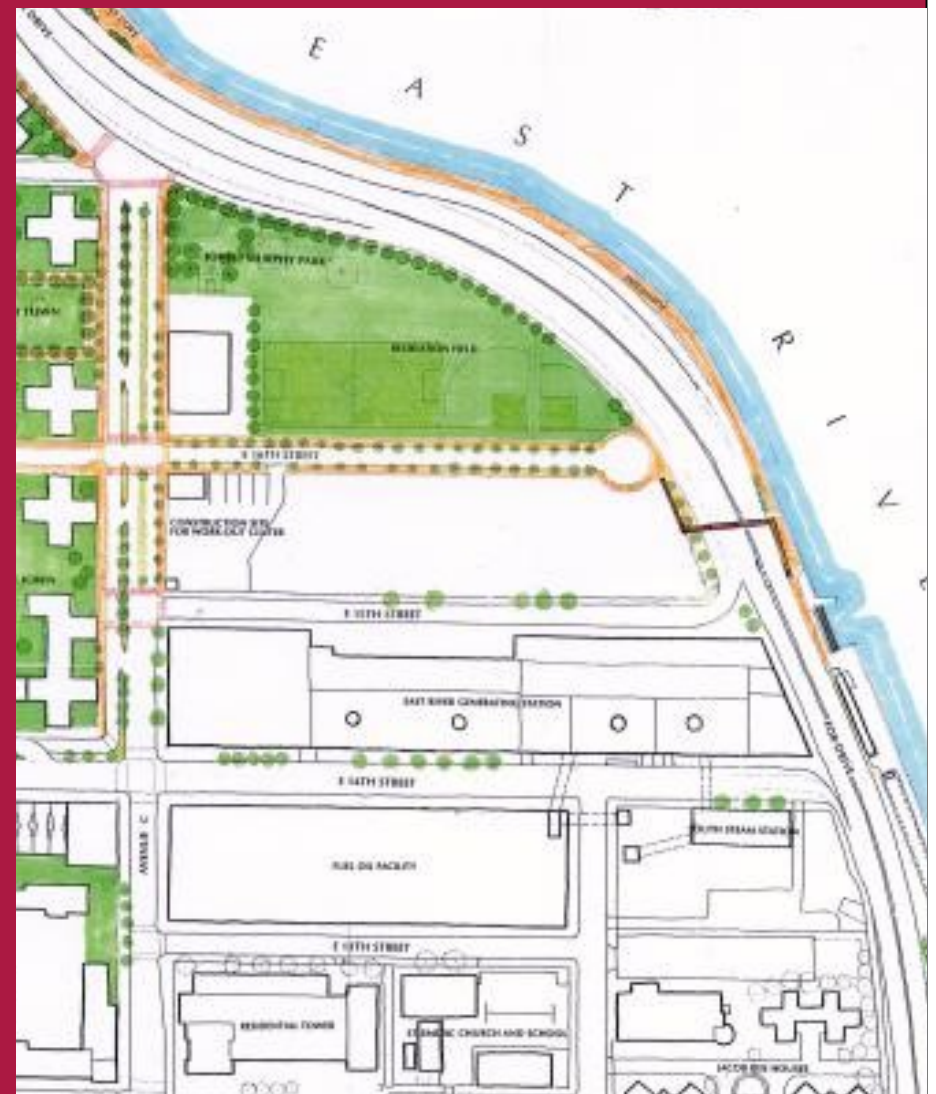


JPW III
30 Jan 12

Existing



Proposed



CB#6 Waterfront 23 Street



There are existing pedestrian crosswalks under the FDR Drive viaduct at and south of 23 Street and there is an existing pedestrian bridge at 25 Street.



JPW III
30 Jan 12



CB#6 Waterfront 25 Street Bridge

Waterside has an existing pedestrian bridge at 25 Street.

The bridge at 25 Street should be modified to provide handicap access at its west end.





26 Street looking west



26 Street looking east



CB#6 Waterfront 26 Street

Although 26 Street looks like a street, east of First Avenue it is not on the City Map. It provides access to the fronting buildings and from the “new” Bellevue building.

Because 26 Street is not mapped it does not exist for the Zoning Resolution and there are therefore no height and setback regulations for the fronting buildings. This is why the building on the north side of the street was able to be built to its full height without any setback from the street. (The buildings on the south side of the street were completed in 1952, before the current Zoning Resolution was adopted in 1961.)

Because 26 Street is not mapped but serves as a driveway on the Bellevue campus it is not necessarily public space and could be gated and closed to the public administratively, without public review, similarly to 28 and 29 Streets.



CB#6 Waterfront 27 Street Bridge

Although Waterside has an existing pedestrian bridge at 25 Street the project was designed to receive a second pedestrian bridge at 27 Street.

The bridge at 27 Street could be built now. It would provide a convenient connection between Bellevue Hospital and the elevated public plaza in Waterside.

Building the bridge should inspire further evolution of the general circulation spaces within the hospital to provide a convenient route between the bridge and First Avenue.

A bridge at 27 Street would also add to the reasons for remapping 26 Street as a public way between First Avenue and the FDR Drive.





28 Street (alley?) looking east



28 Street looking east



28 Street looking west



28 Street looking north

CB#6 Waterfront 28 Street

27 and 28 Streets were eliminated east of First Avenue and occupied by the old Bellevue complex. As buildings have been demolished and built 28 Street has been reopened, although off axis, as a drive for ambulances to the emergency room. It is a shorter route than the one that had been planned via 29 Street and may save lives.

Because 26 Street is not on the City Map but serves as a driveway on the Bellevue campus it is not necessarily public space and is gated and could be closed to the public administratively, without public review, similarly to 26 and 29 Streets.

CB#6 Waterfront 29 Street Bridge

The Bellevue Science Park is being built with its ground level elevated on a deck at approximately the level of First Avenue and a story above the FDR Drive and its service roads.

In the future, when the FDR Drive is rebuilt and the south-bound lanes brought to grade further north, a pedestrian bridge or landscaped deck can be built above the FDR Drive connecting 29 Street and the Bellevue Science Park to the esplanade along the East River.





29 Street looking west



29 Street looking east



CB#6 Waterfront 29 Street

29 Street east of First Avenue is not on the City Map.

Because 29 Street is not mapped it does not exist for the Zoning Resolution and there are therefore no height and setback regulations for the fronting buildings. This is why the building on the south side of the street near the FDR Drive was able to project into the street and be built to its full height without any setback from the street.

Because 29 Street is not on the City Map but serves as a driveway on the Bellevue campus it is not necessarily public space and is gated and could be closed to the public administratively, without public review, similarly to 26 and 29 Streets.

However, 29 Street has been rebuilt rising slightly from First Avenue to the FDR Drive so as to allow a future pedestrian bridge to span the highway to the esplanade.

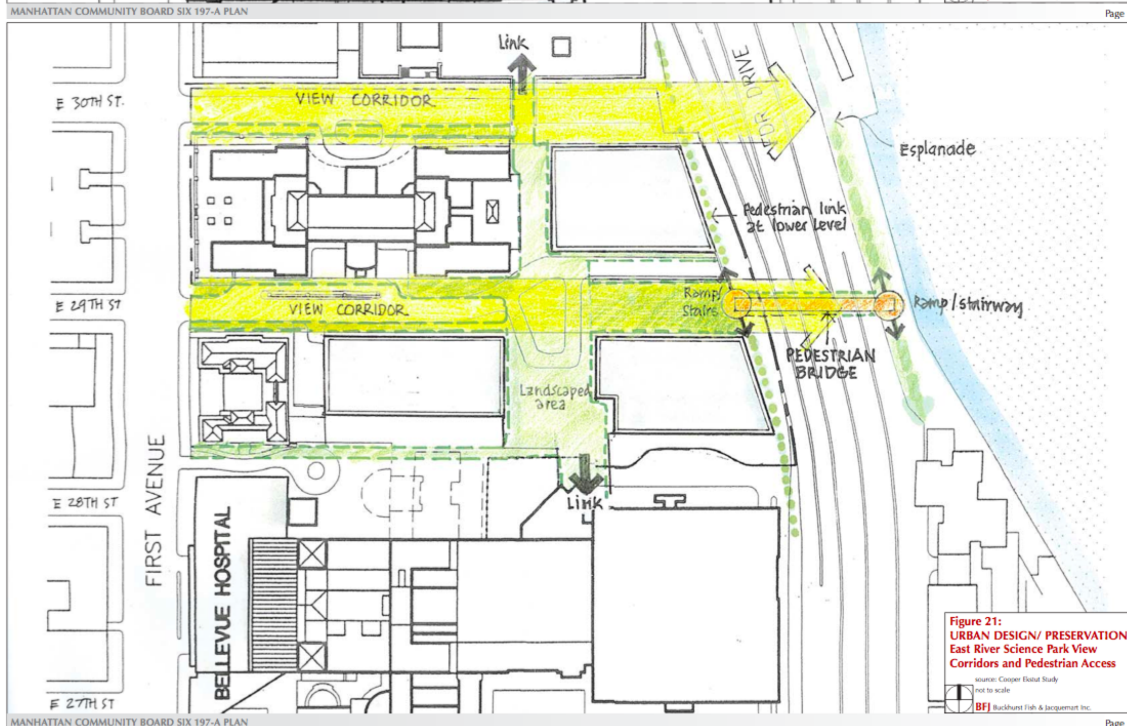
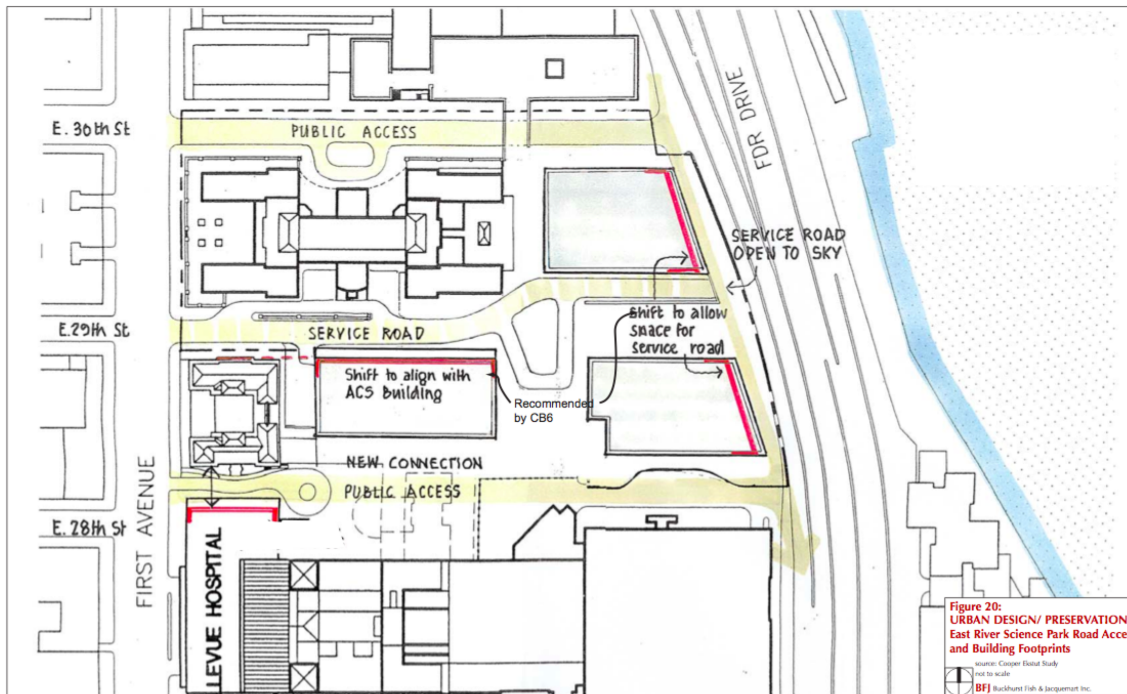
CB#6 Waterfront

East River Science Park

Following a series of meetings during the summer of 2001 to improve the site plan for the East River Science Park conditionally withdrew its opposition.

The main public goals of the revised site plan included:

- Keeping 29 and 30 Streets open as visual corridors to the East River,
- Providing public circulation along 28, 29, and 30 Streets,
- Providing ambulance access to Bellevue along 28 Street,
- Having 29 Street slope up slightly from First Avenue to the FDR Drive to allow a future pedestrian bridge to span the highway to the esplanade,
- Providing north-south pedestrian circulation at mid-block between Bellevue and NYU, and
- Including a substantial public open space at the center of the complex.





30 Street looking west



30 Street looking east

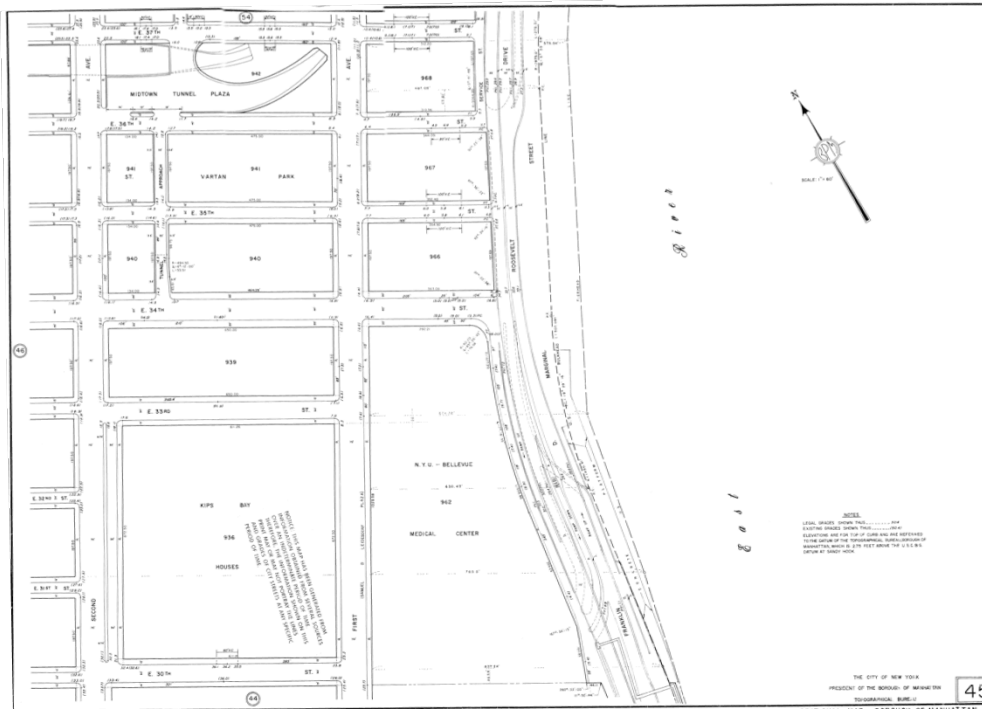


CB#6 Waterfront 30 Street

30 Street east of First Avenue is not on the City Map.

When a street is not mapped it does not exist for the Zoning Resolution. It therefore becomes part of the lot area of the adjacent sites and is included in the zoning floor area that is allowed and because there are no street lines there are no height and setback regulations for the fronting buildings.

30 Street functions as a street and is needed as a street. It should be remapped.



37 St

36 St

35 St

34 St

33 St

32 St

31 St

30 St

30 St

29 St

28 St

27 St

26 St

25 St

24 St

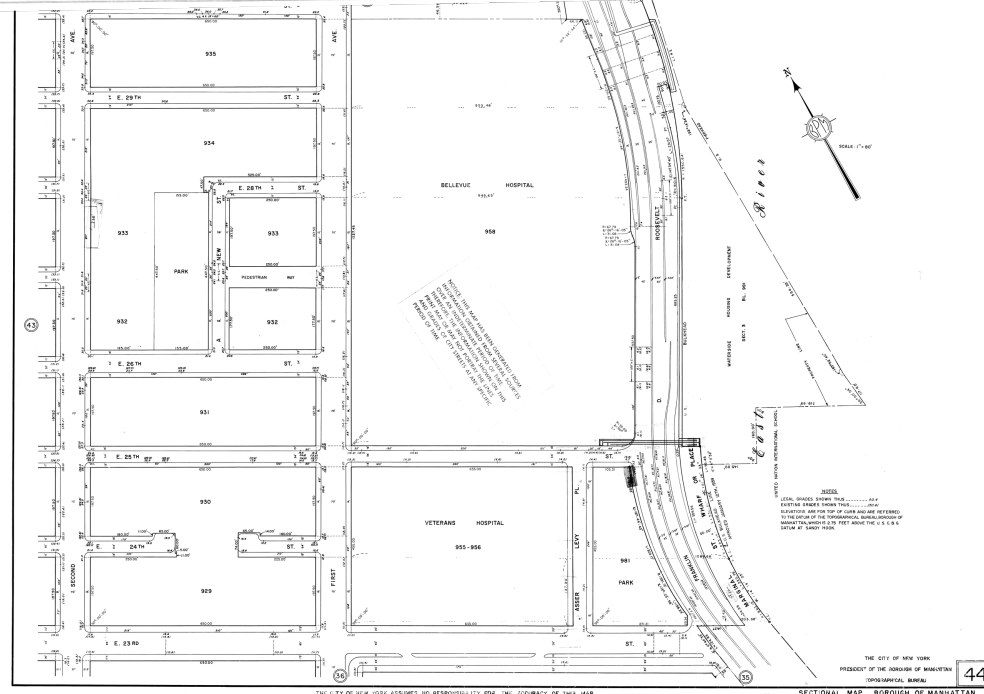
23 St

CB#6 Waterfront 30 Street

30 Street east of First Avenue is not on the City Map.

Except for 25 Street all of the streets between 23 and 34 Streets were closed east of First Avenue to make large sites for institutions such as Bellevue and NYU at a time when the public had little interest in visiting the river. However, several streets, particularly 30 Street, physically remain.

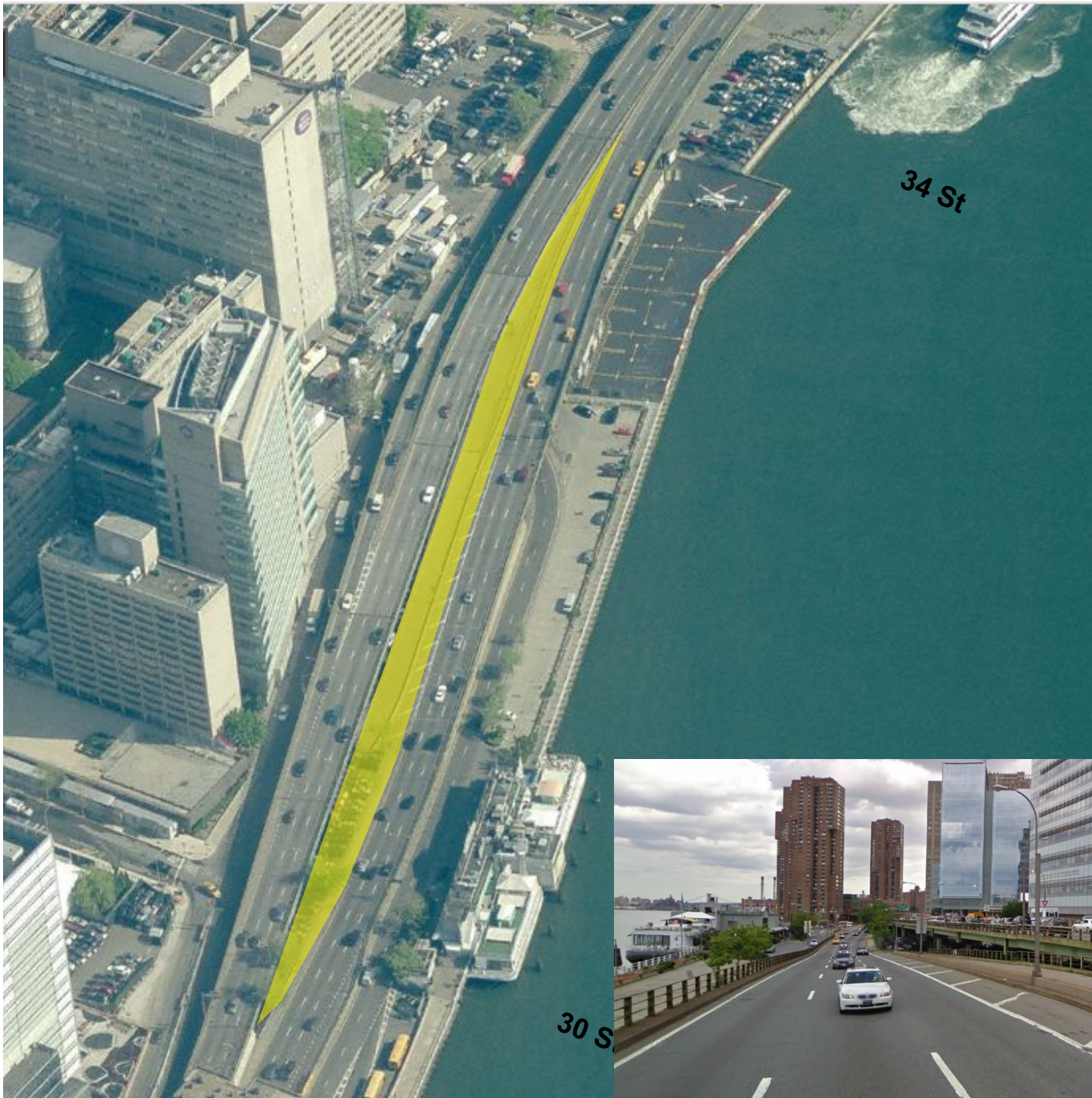
Times change and now the public interest is served by access to and along the East River. Because 30 Street functions, most of the time, as a street returning it to the City Map ought to be easy.



CB#6 Waterfront 30 to 34 Streets

The FDR Drive between 30 and 34 Streets is designed to accommodate a future ramp to the once planned expressway across Midtown Manhattan. As a result the southbound lanes remain elevated as far south as 28 Street to allow the ramps to pass under them. The space between the northbound and southbound lanes between 30 Street and 33 Street would have been the connecting ramp.

When this portion of the FDR Drive is rebuilt the elevated portion should be shortened, allowing pedestrian bridges at 29 Street and perhaps at 31 Street, and the highway should be narrowed, removing the area occupied by the unused ramp and widening the esplanade.

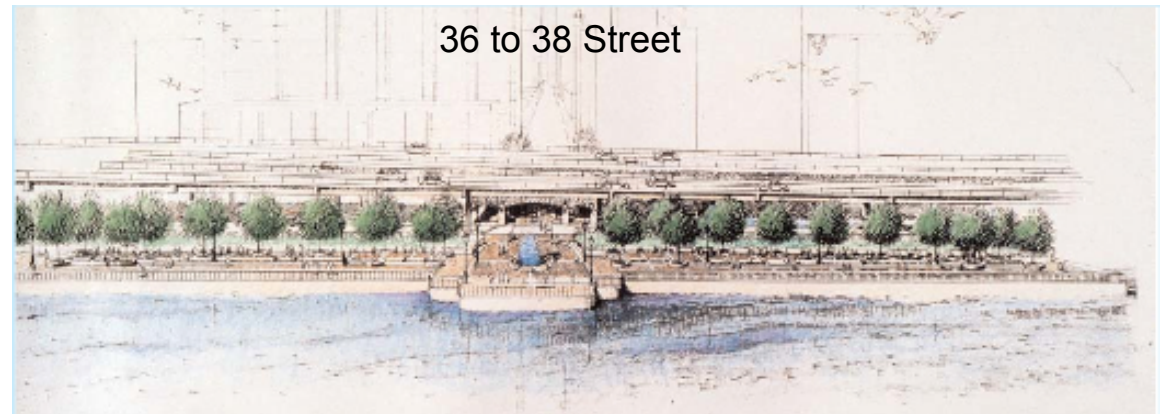




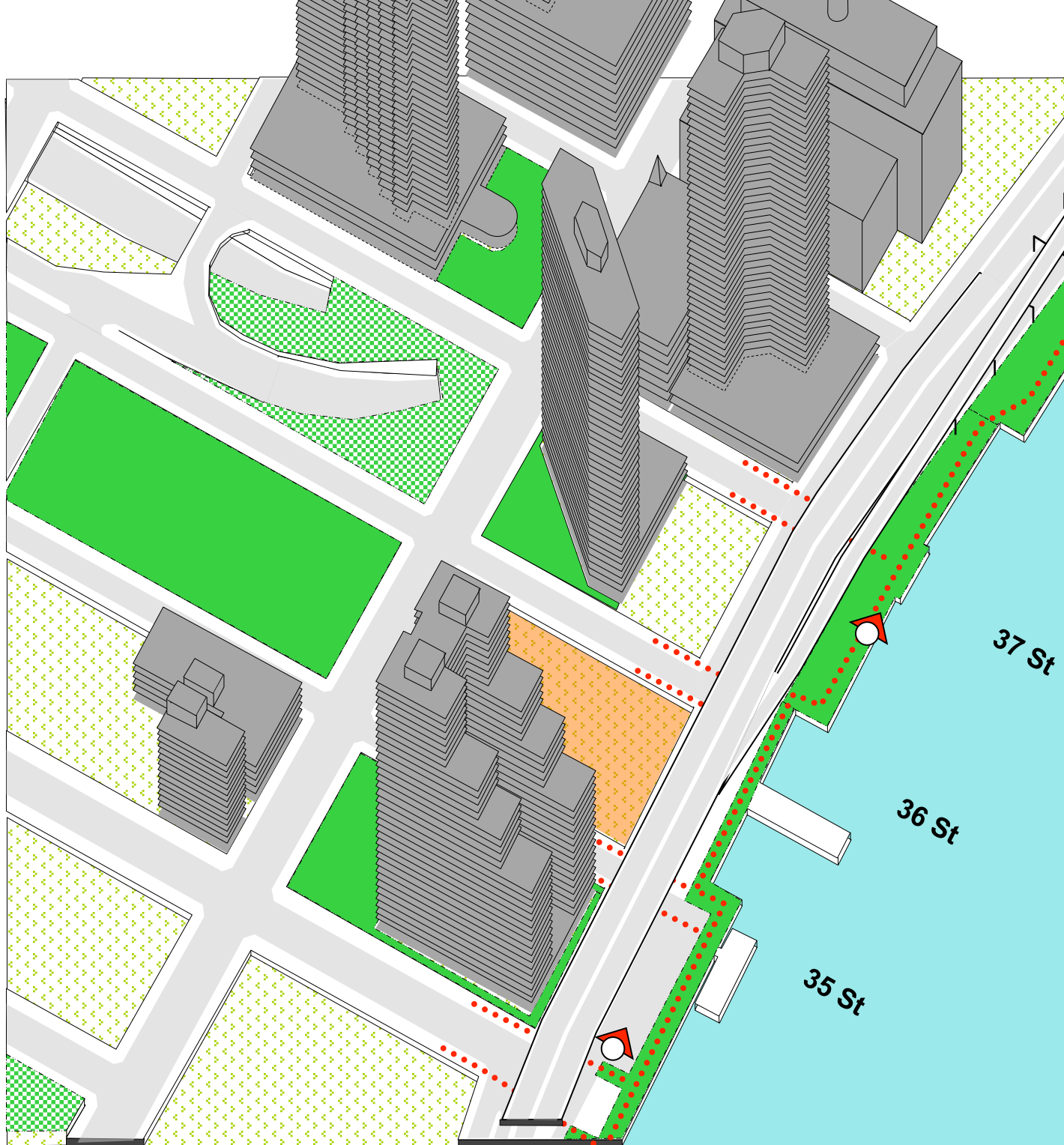
CB#6 Waterfront
34 to 38 Streets



Foot of 34 Street



36 to 38 Street



CB#6 197a Plan

34 to 38 Streets

The existing FDR Drive is on grade north of 38 Street and is on a viaduct south of 36 Street. Between 38 and 36 Streets it is on a ramp with increasing head room.

At 37 Street there is just enough height for a pedestrian passage at grade between the waterfront and the upland.

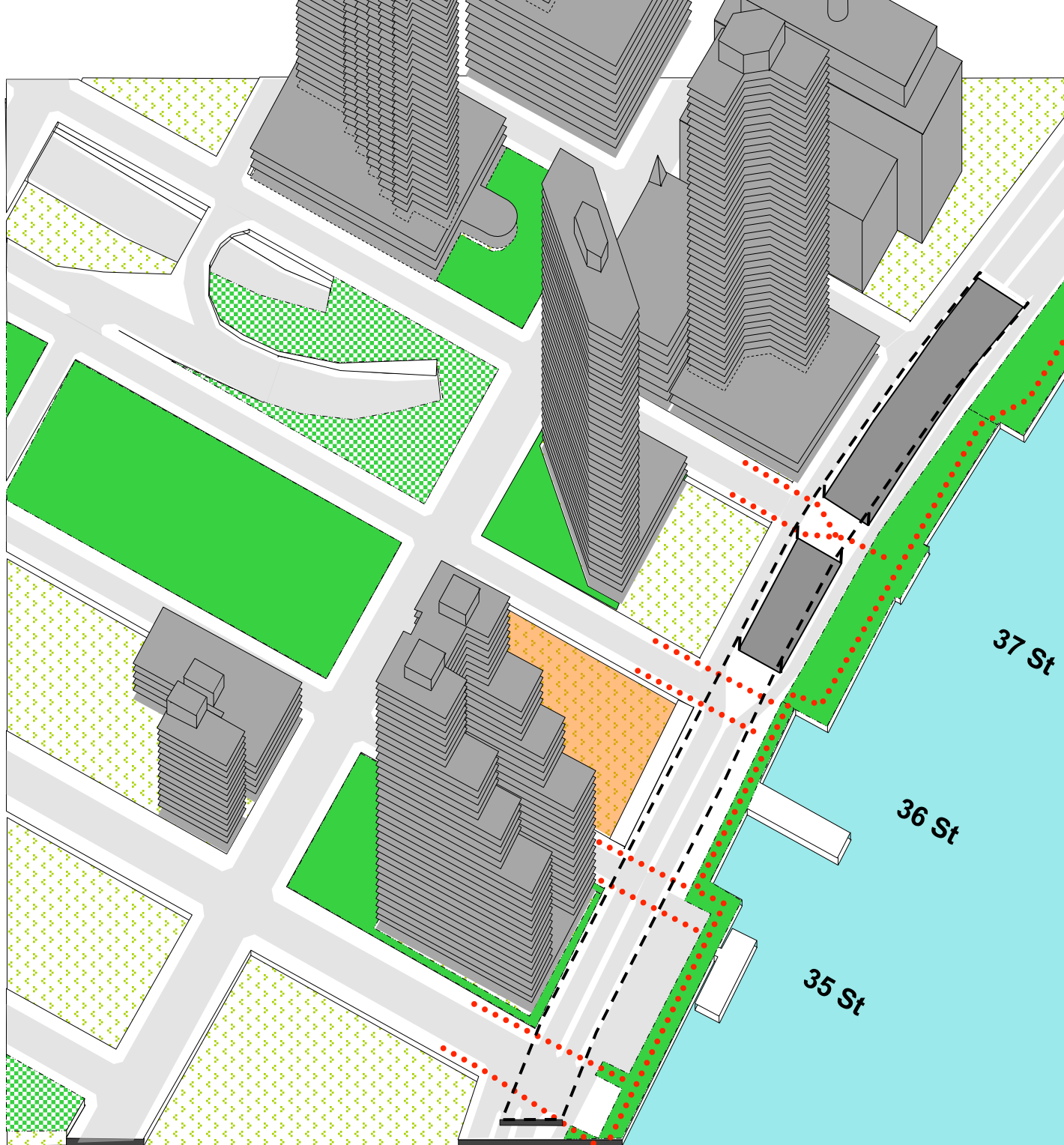
South of 36 Street there is enough height for the service roads to be under the main line rather than along side.

The northbound exit ramp begins at 36 Street, where the viaduct is at its full height, and continues at that height to 42 Street.

In order to provide pedestrian access over the FDR Drive between 38 and 41 Streets the ramp needs to be shortened or removed.

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CB#6 197a Plan

34 to 38 Streets

The viaduct spanning 34 Street is as long as it is because the right of way is not wide enough south of 36 Street for the south bound service road to be along side rather than under the main line.

If an easement were granted along the east edge of the former Con Ed property between 35 and 36 Streets the southbound service road and the north bound entrance ramp to the FDR Drive could be along side, rather than under, the main line for an additional block.

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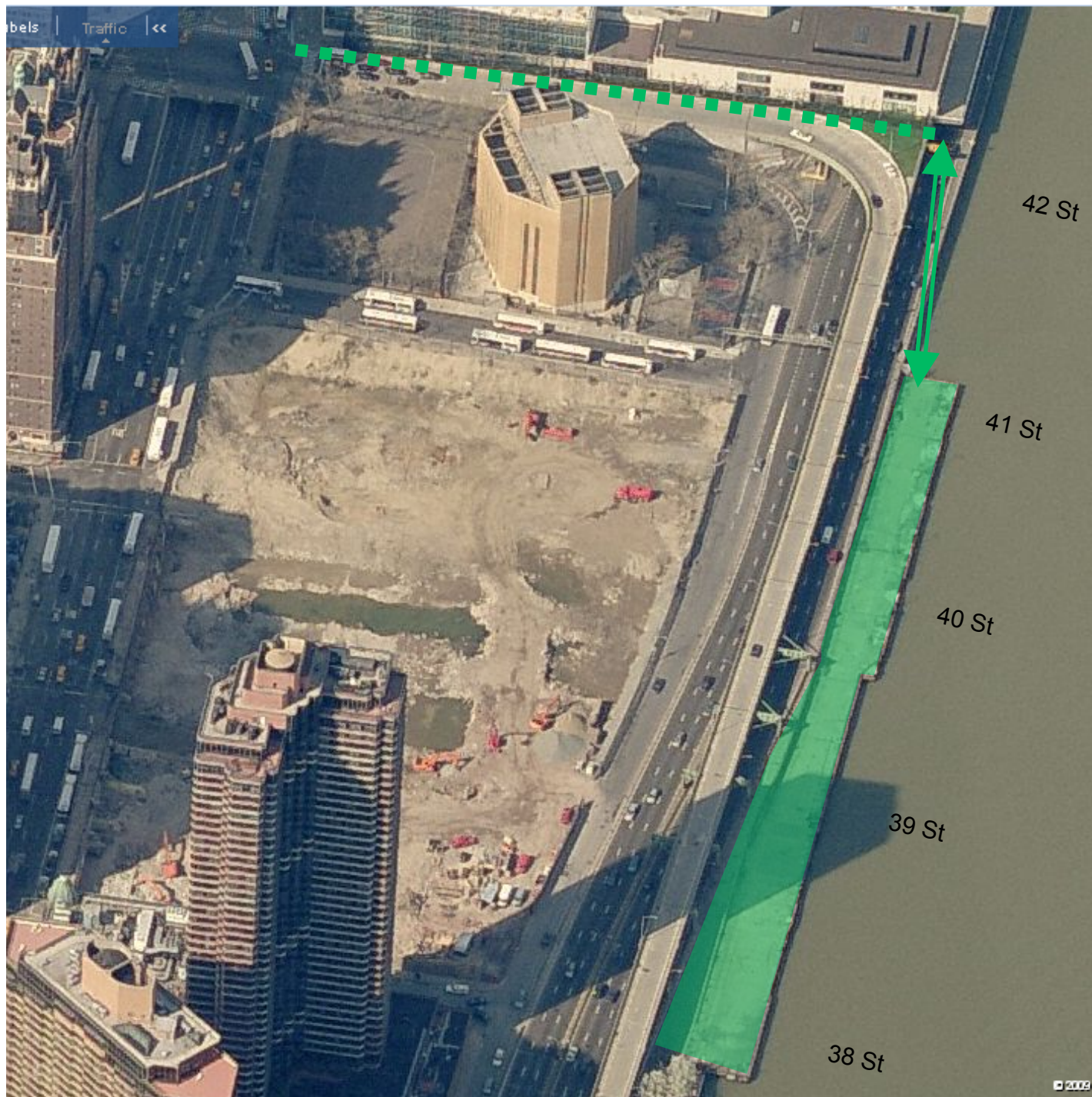
This aerial view shows a city block with a proposed pedestrian route highlighted in red dots. The route starts near 35 St and runs parallel to the waterfront. Buildings are shown in gray, and green spaces are in green. The waterfront area is labeled 35 St, 36 St, and 37 St.

On the other hand, a shorter viaduct would eliminate the pedestrian access to the waterfront at 37 Street and would narrow the open space along the river between 35 and 36 Streets.

There would be enough height under the viaduct at 36 Street for a pedestrian crossing and enough height at 35 Street for vehicles, including buses stopping at the ferry landing.

Because the northbound entrance ramp would join the main line sooner the esplanade might be wider north of 38 Street.

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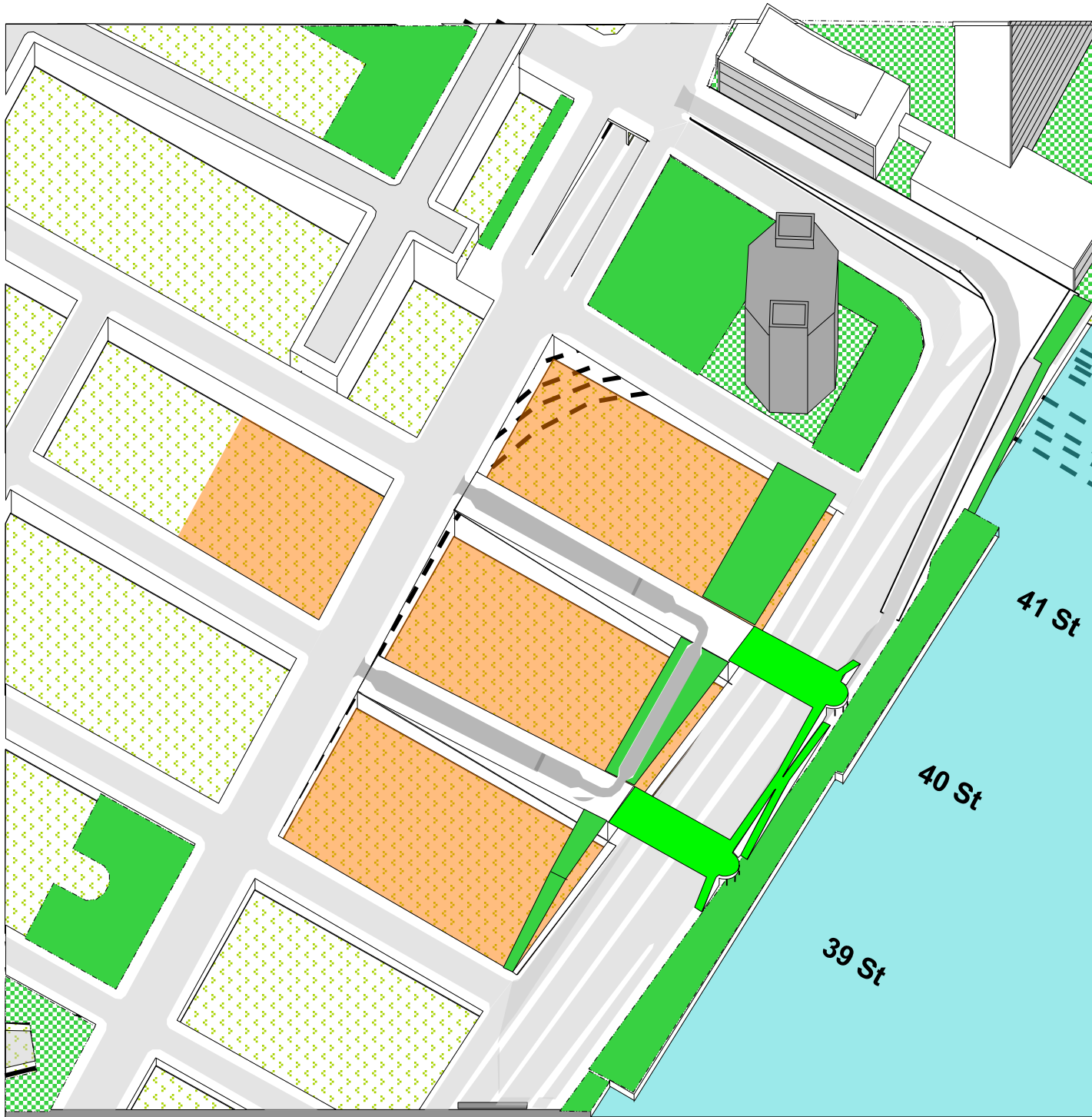


CB#6 Waterfront 42 Street Ramp

The City-owned pier along the edge of the East River between 38 and 41 Streets was formerly used by Con Ed, most recently for parking, but is being returned to City control in the near future and is expected to be landscaped as part of the esplanade along the river.

In the future, when the FDR Drive is rebuilt with its north-bound exit ramp lowered and the Con Ed site is redeveloped with its ground level elevated on a deck at approximately the level of First Avenue and a story above the FDR Drive and its service road, pedestrian bridges or a landscaped deck can be built above the FDR Drive connecting 39 and 40 Streets to the esplanade along the East River.

In the meantime, a pedestrian and bicycle ramp is needed to connect the new section of esplanade to 42 Street.



CB#6 197a Plan

38 to 42 Streets

The city owned pier along the river between 38 and 41 Street plus a ramp to the foot of 42 Street can complete the esplanade between 34 and 42 Streets.

The redesign of the FDR Drive needs to shorten or eliminate the northbound exit ramp to 42 Street in order to allow access to the waterfront.

However, eliminating the exit ramp to 42 Street allows the highway to be narrower and the esplanade to be wider.

The site plan for the Con Ed properties includes:

- Rebuilding 39 and 40 Streets to be one level above grade at the FDR Drive.

- A promenade along the east edge of the Con Ed blocks.

- 39 and 40 Streets can be extended across the FDR Drive and connected to the esplanade.

- An easement along the east edge of the Con Ed blocks allows the FDR Drive to be aligned further west allowing the esplanade to be wider.

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CB#6 197a Plan

38 to 42 Streets

Rebuilding 39 and 40 Streets, providing a promenade overlooking the FDR Drive, completing the esplanade between 38 and 42 Streets and bridging the FDR Drive at 39 and 40 Streets might be part of the redevelopment of the Con Ed properties.

Adding landscaped decks above the FDR Drive between 38 and 41 Streets might be part of the rebuilding of the FDR Drive or part of the redevelopment of the Con Ed properties or part of both.

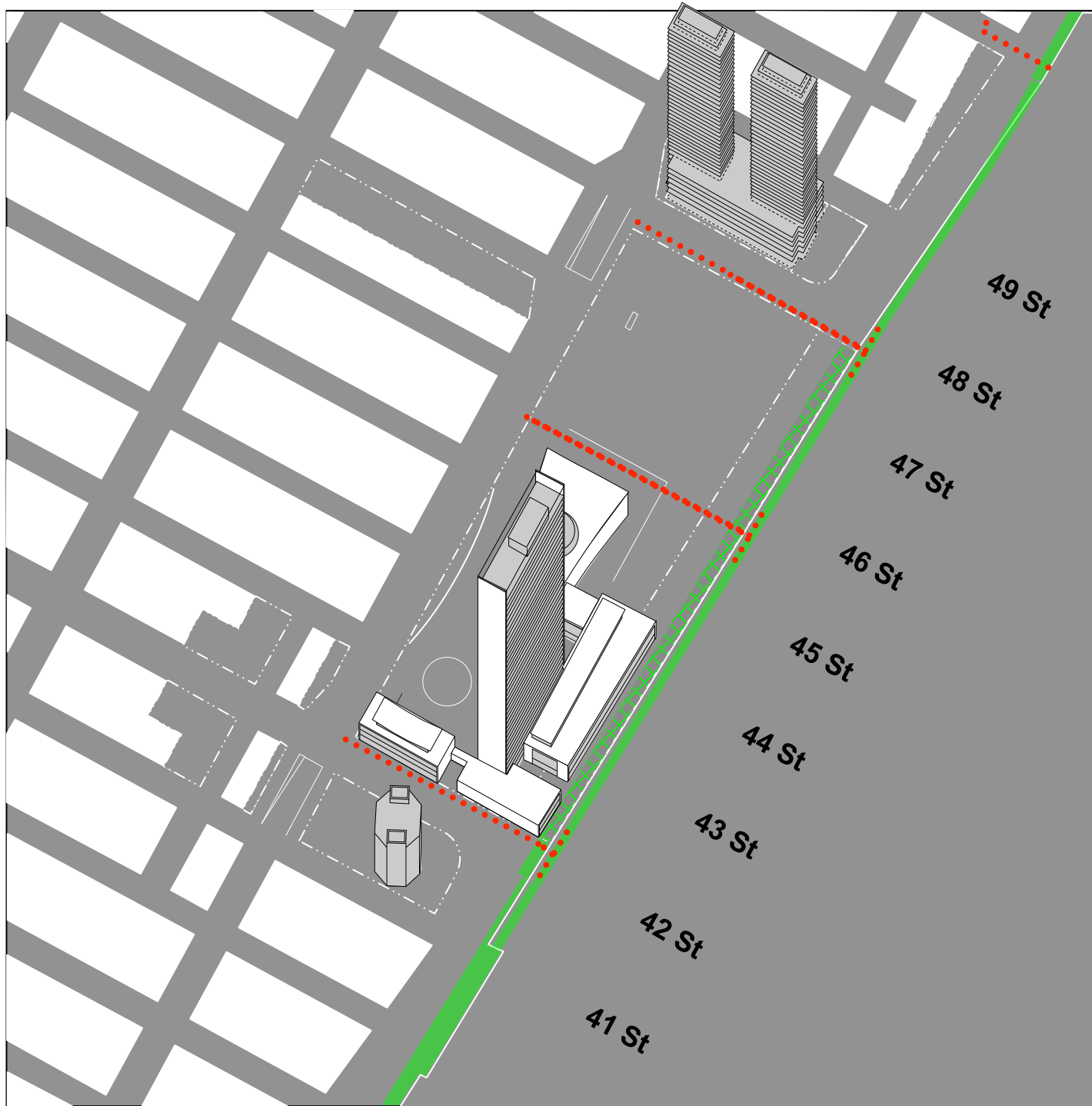
Widening the 42 Street ramp between First Avenue and the East River to provide a cross-town transit terminus convenient to a ferry landing might be part of rebuilding the FDR Drive.

Adding a deck above the FDR Drive between 41 and 42 Streets, possibly including an area above the east end of Robert Moses Playground, might be part of the rebuilding of the FDR Drive or part of the proposed new building for the United Nations.

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United Nations Esplanade

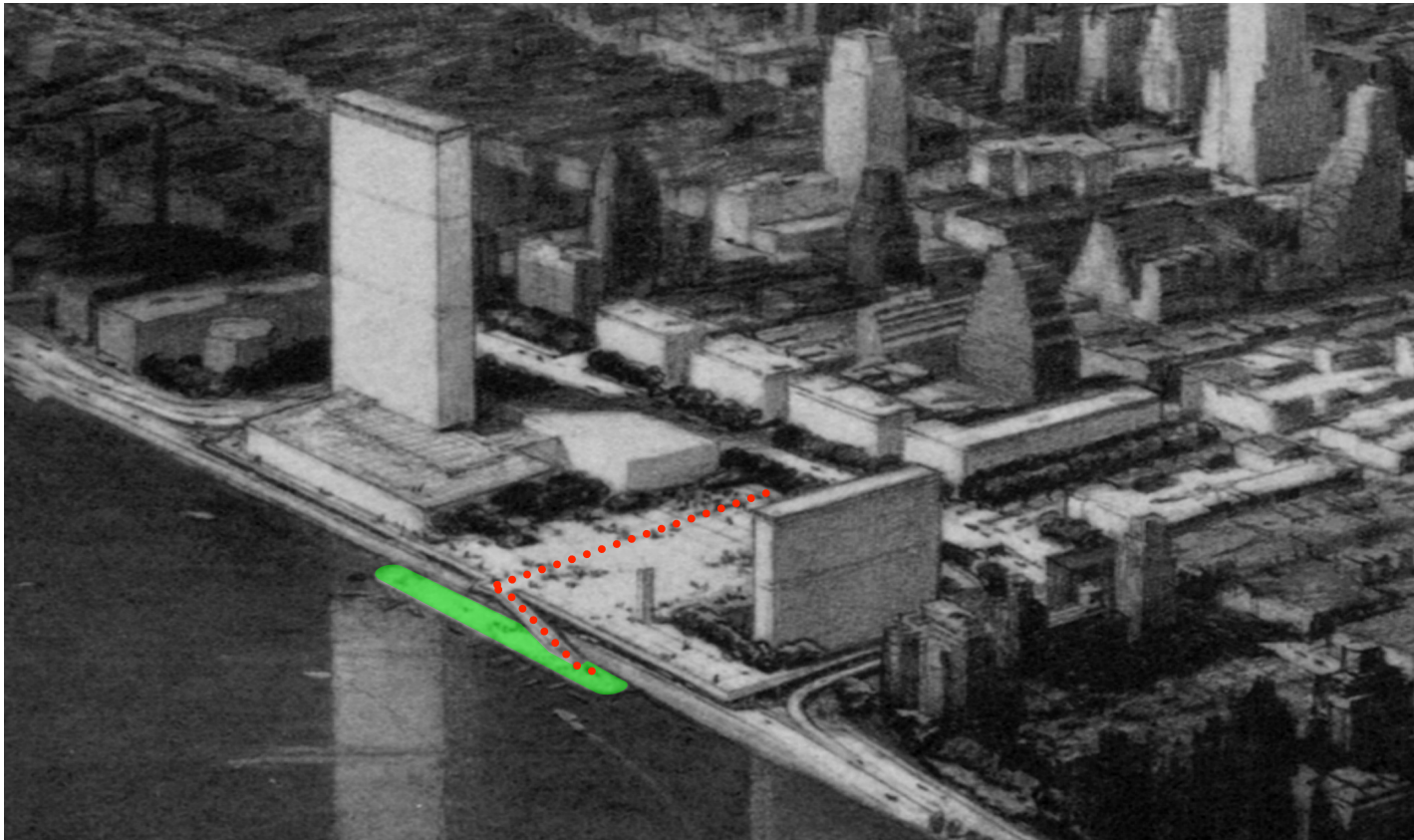
It appears, for reasons of security, that the existing esplanade at the United Nations is unable to serve as part of the continuous public esplanade planned around the island of Manhattan.

An alternative link between 42 and 48 streets might be built along the east edge of the FDR Drive, 20 feet below and separated from the existing esplanade. It would need to extend to 51 Street and to 41 Street.

Pedestrian access would be provided across the FDR Drive and down to the esplanade at 42 and 48 Streets.

An additional point of pedestrian access might be provided through the UN grounds although one would expect it to be closed often for security reasons.

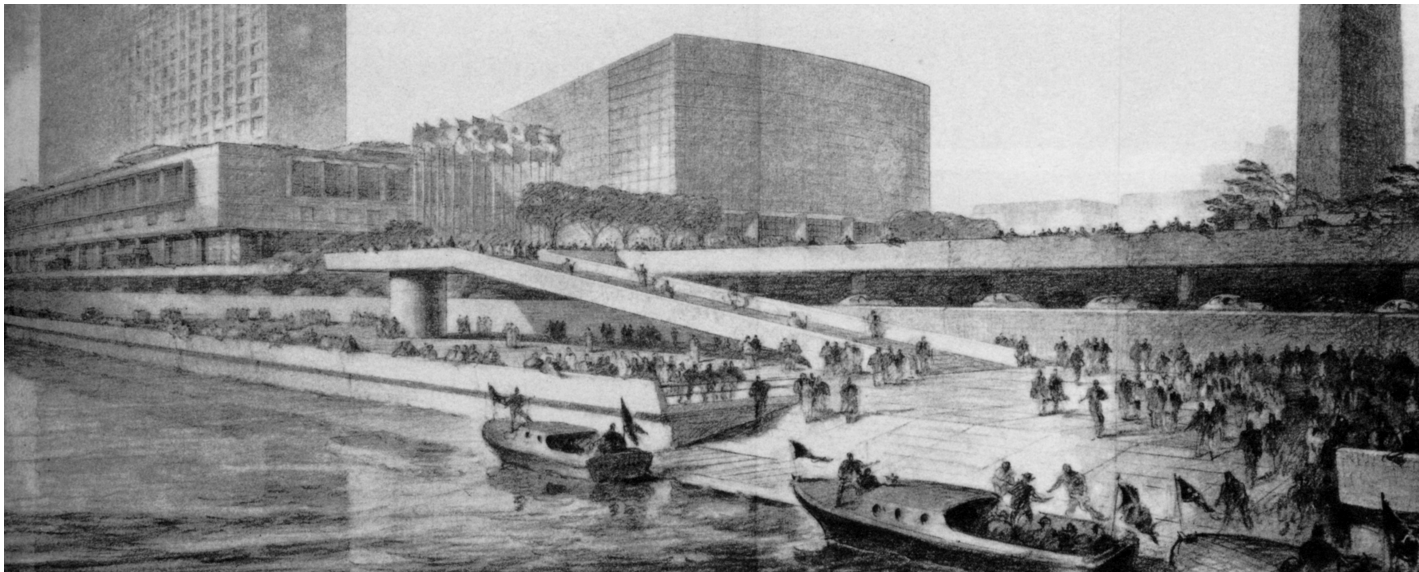
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CB#6 Waterfront United Nations Watergate

The design for the United Nations – the workshop for peace – as approved in 1947 reached out to its surroundings – west along 47 Street toward Midtown and east to a boat landing on the river, welcoming crowds of people.

Perhaps it is not too much to hope for a revival of that optimism and the eventual provision of access through the United Nations campus to the esplanade.

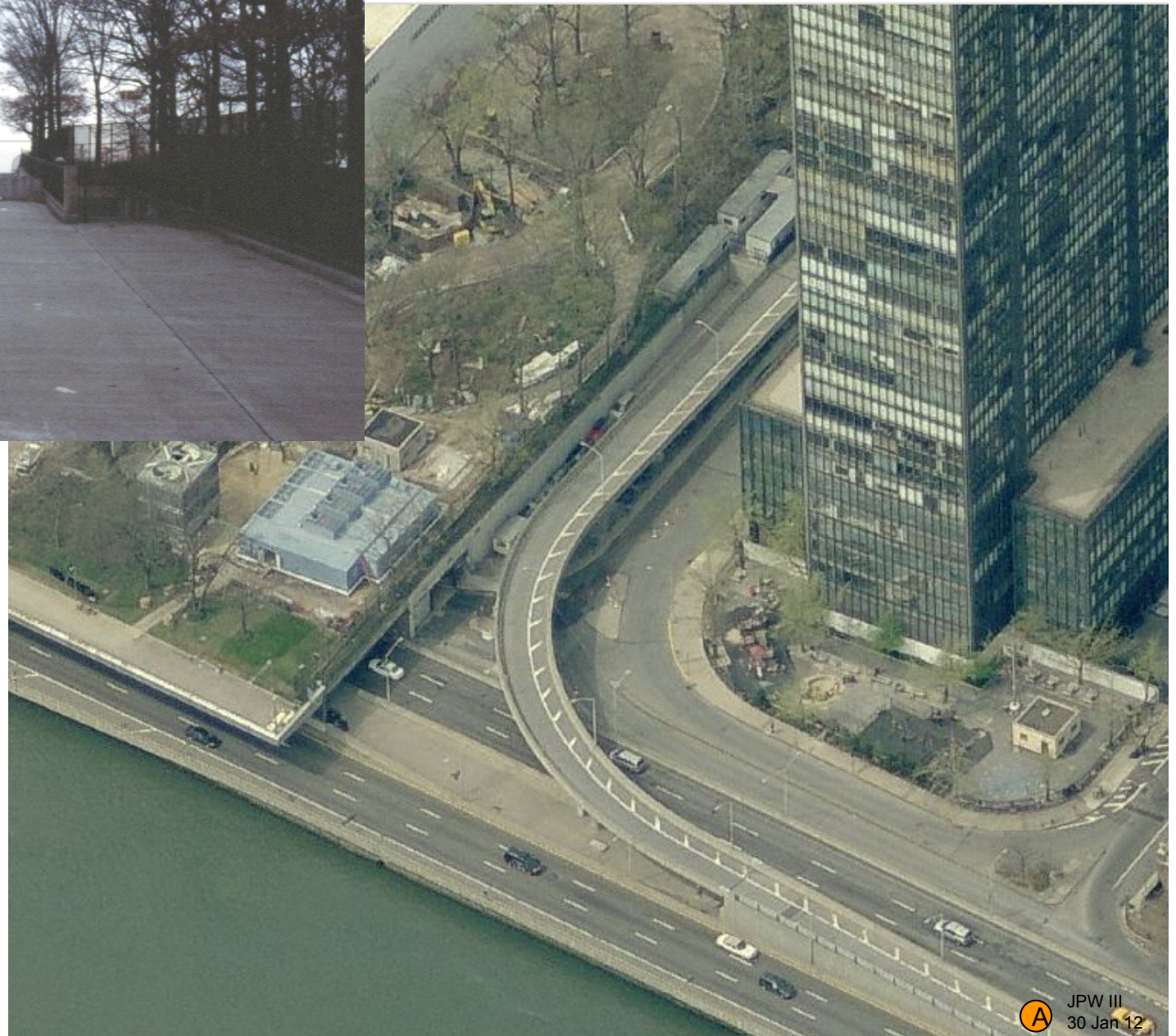


CB#6 Waterfront To Esplanade at 48 Street



When the northbound entrance ramp to the FDR Drive at 48 Street was rebuilt a slot of space was left between the ramp and the edge of the United Nations for a future pedestrian bridge to the esplanade.

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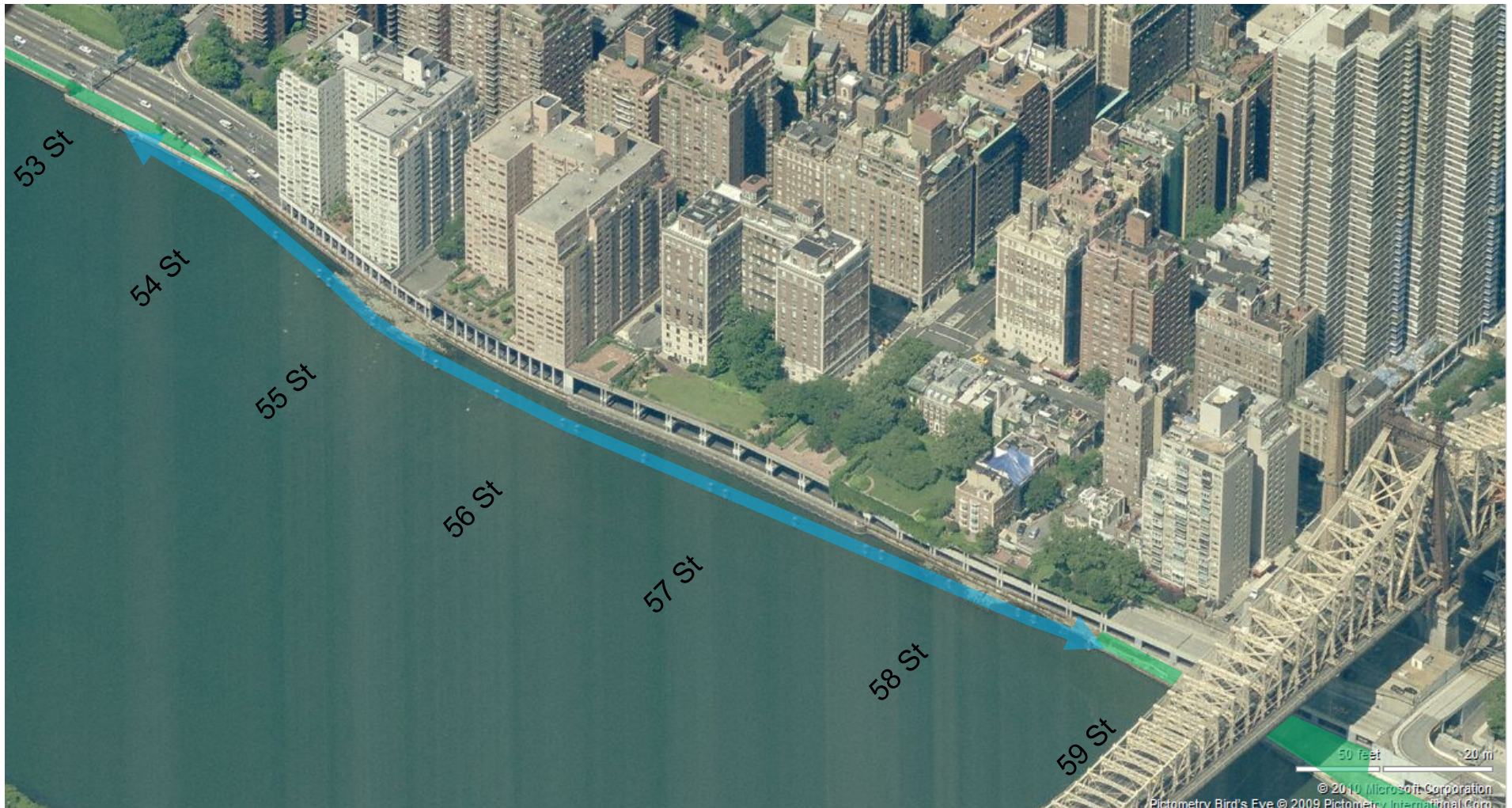
CB#6 Waterfront 51 to 54 Streets

The existing segment of esplanade between 51 and 54 Streets can only be reached by the pedestrian bridge at 51 Street. There have been proposals for an additional pedestrian bridge at 54 Street, just south of where the FDR Drive goes under the apartment buildings.

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CB#6 Waterfront

ODR – 53 to 59 St



The inner row of caissons from the Outer Detour Roadway, used for the reconstruction of the FDR Drive, has been retained to support the missing piece of esplanade around Manhattan between 53/54 and 59/60 Streets.

The green tone shows existing esplanade south of 54 Street and north of 59 Street.

The blue arrow shows the esplanade above the inner row of caissons.

55, 56, 57, and 58 Streets span the FDR Drive but end 40 to 45 feet above the proposed esplanade.

Part of the green area above the FDR Drive between 56 and 57

Streets is being developed as a public park.

CB#6 Waterfront 58 to 60 Streets

59 St

A pedestrian bridge at 59 Street could provide a convenient connection between the esplanade and the Queensboro Bridge for bicycles.

Is Riverview Terrace, between 58 and 59 Streets a mapped City street?

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58 St