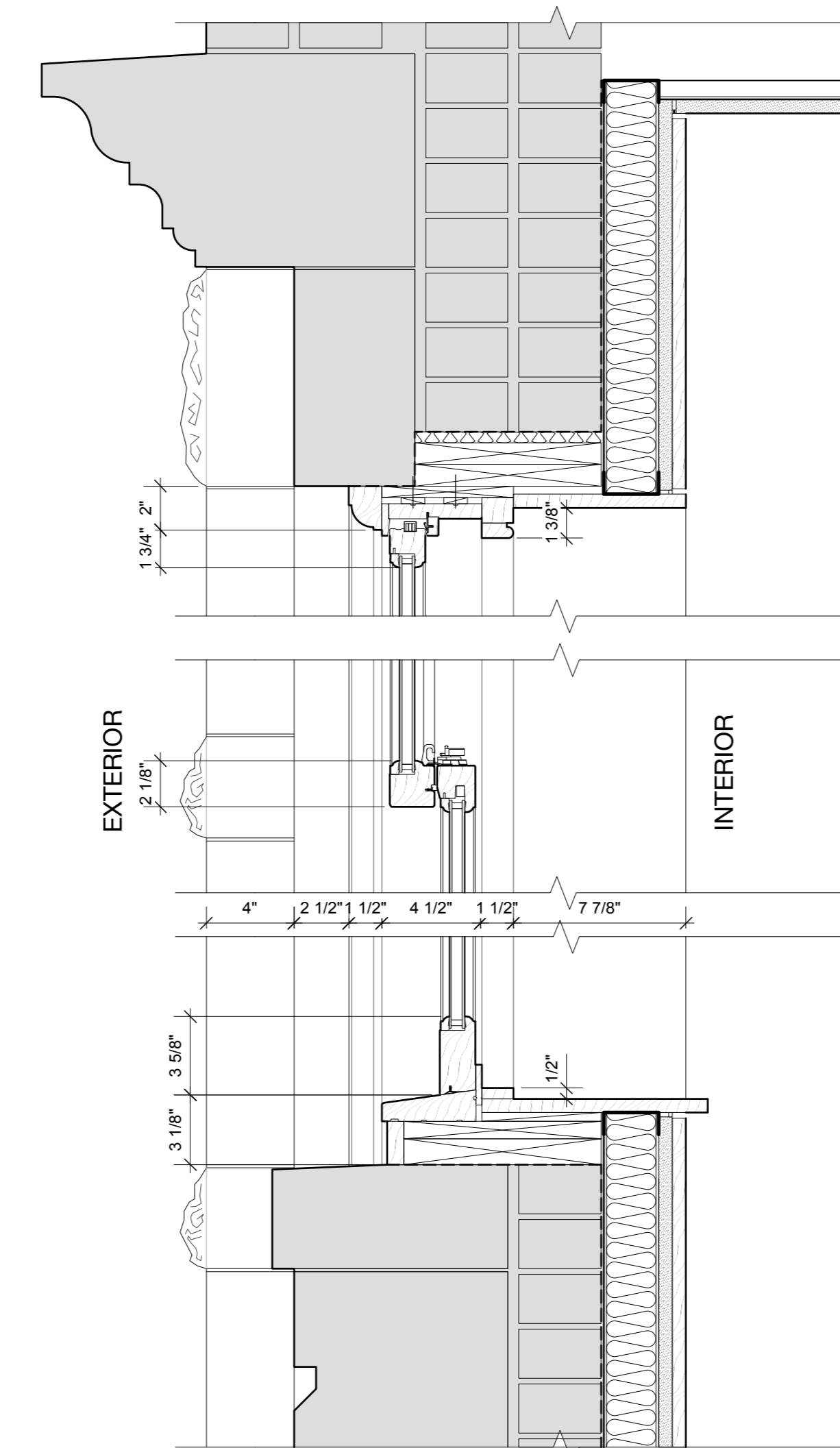
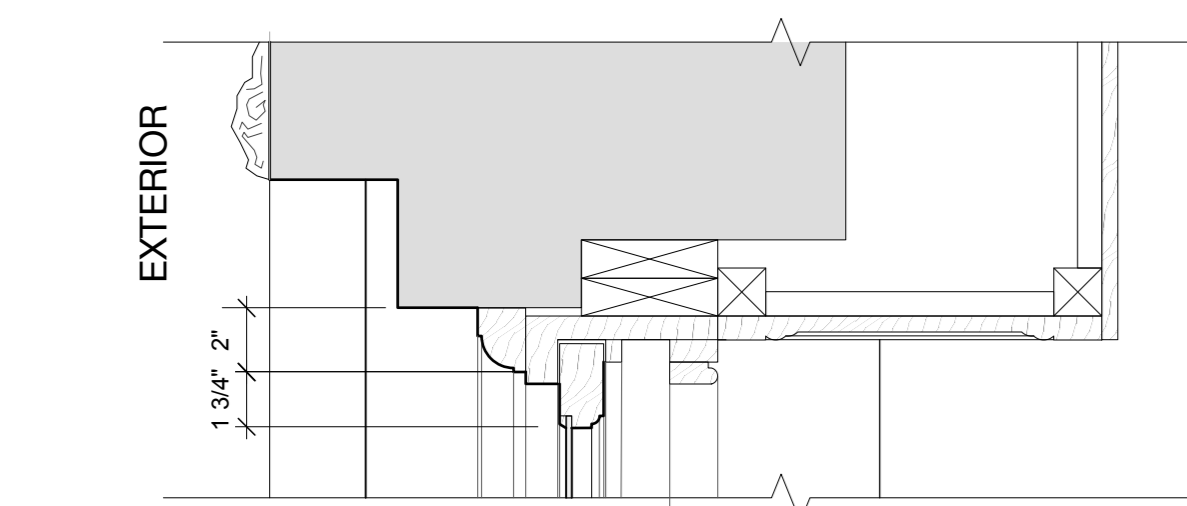


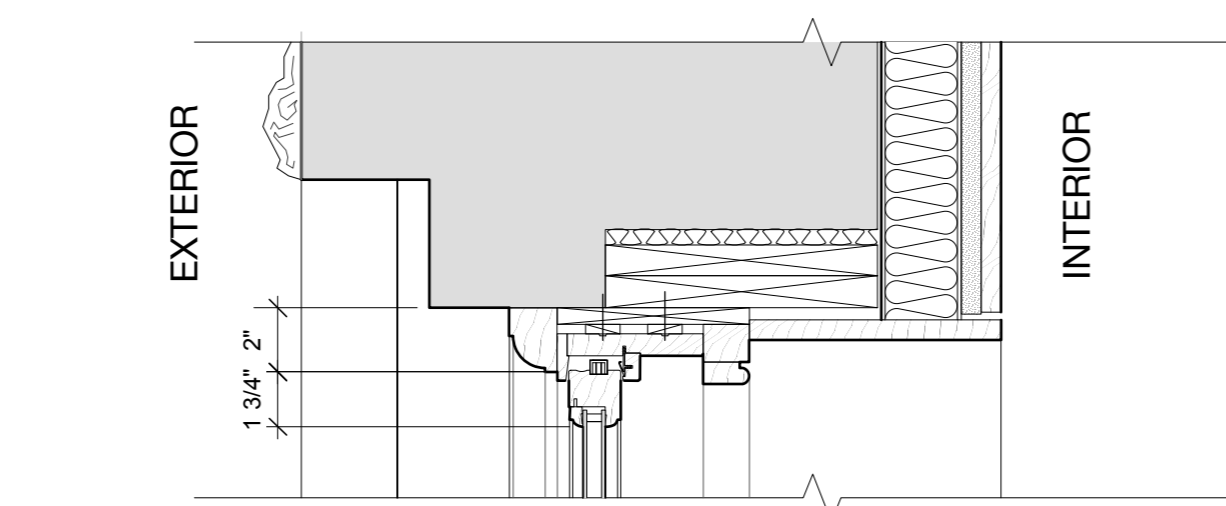
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2" = 1'-0"



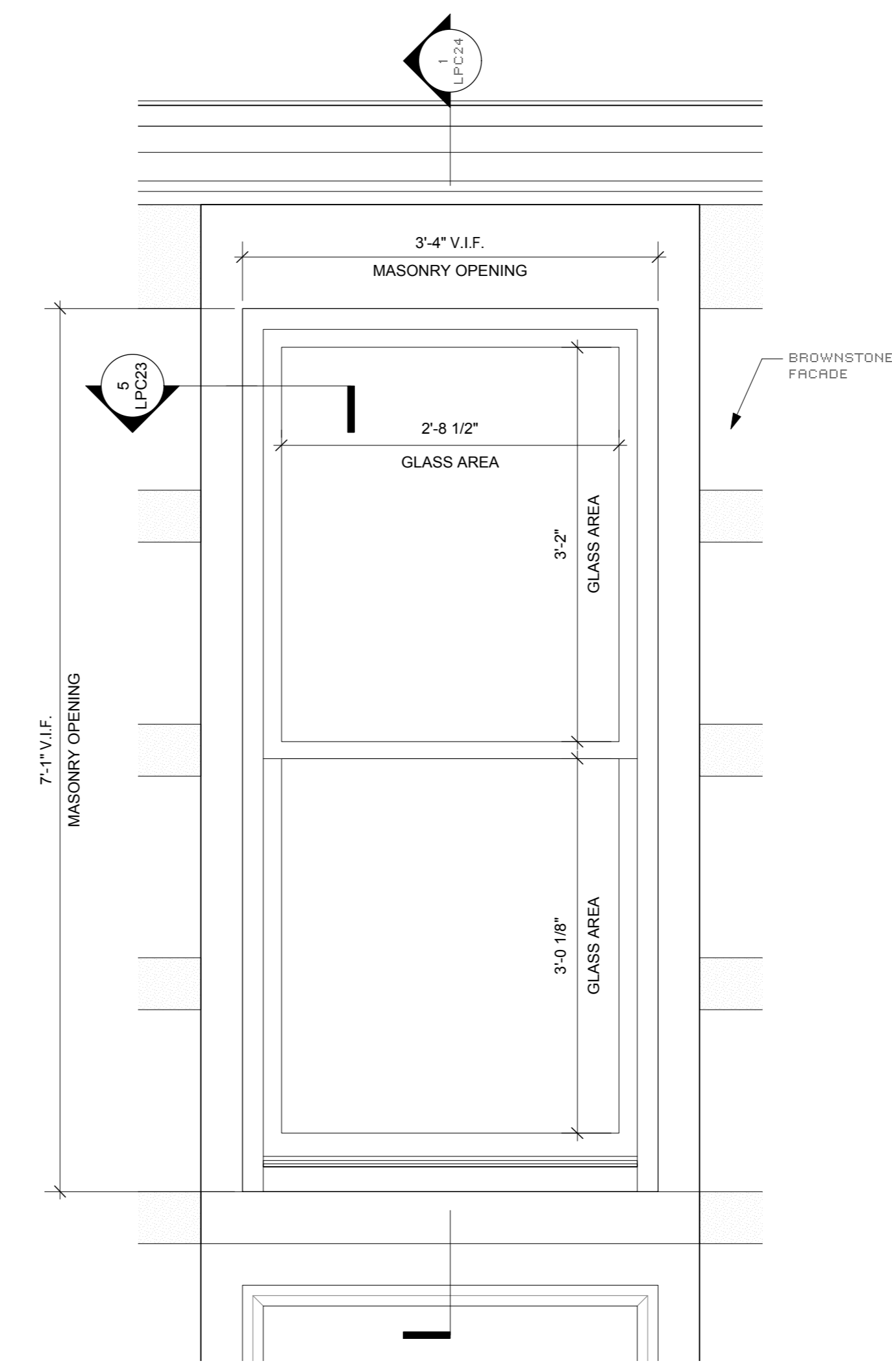
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2" = 1'-0"



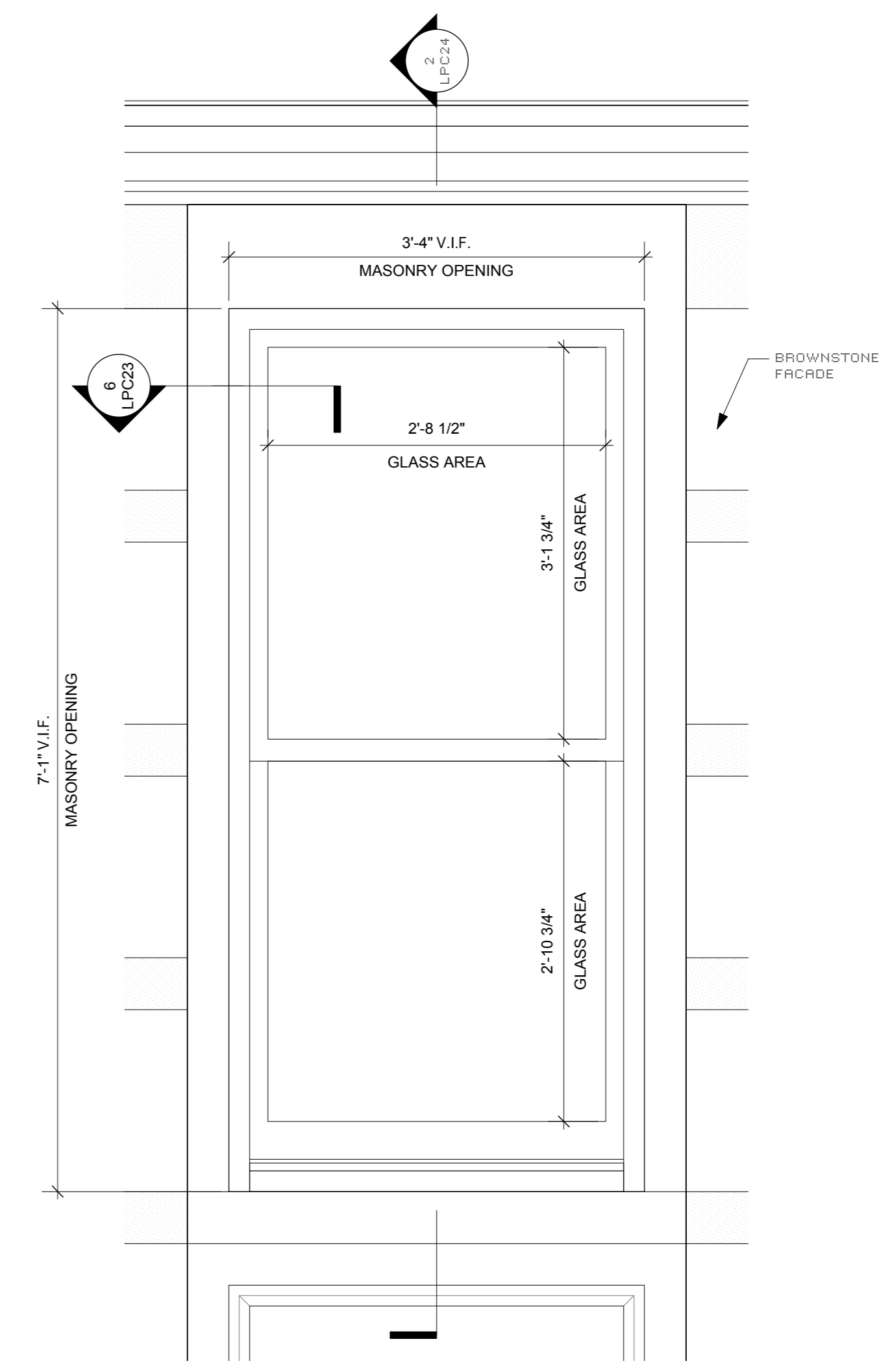
5 TYP. EXISTING WINDOW JAMB @ FIRST FLOOR
2" = 1'-0"



6 TYP. PROPOSED WINDOW JAMB @ FIRST FLOOR
2" = 1'-0"



3 EXISTING ELEVATION: FIRST FLOOR WINDOW
1" = 1'-0"



4 PROPOSED ELEVATION: FIRST FLOOR WINDOW
1" = 1'-0"

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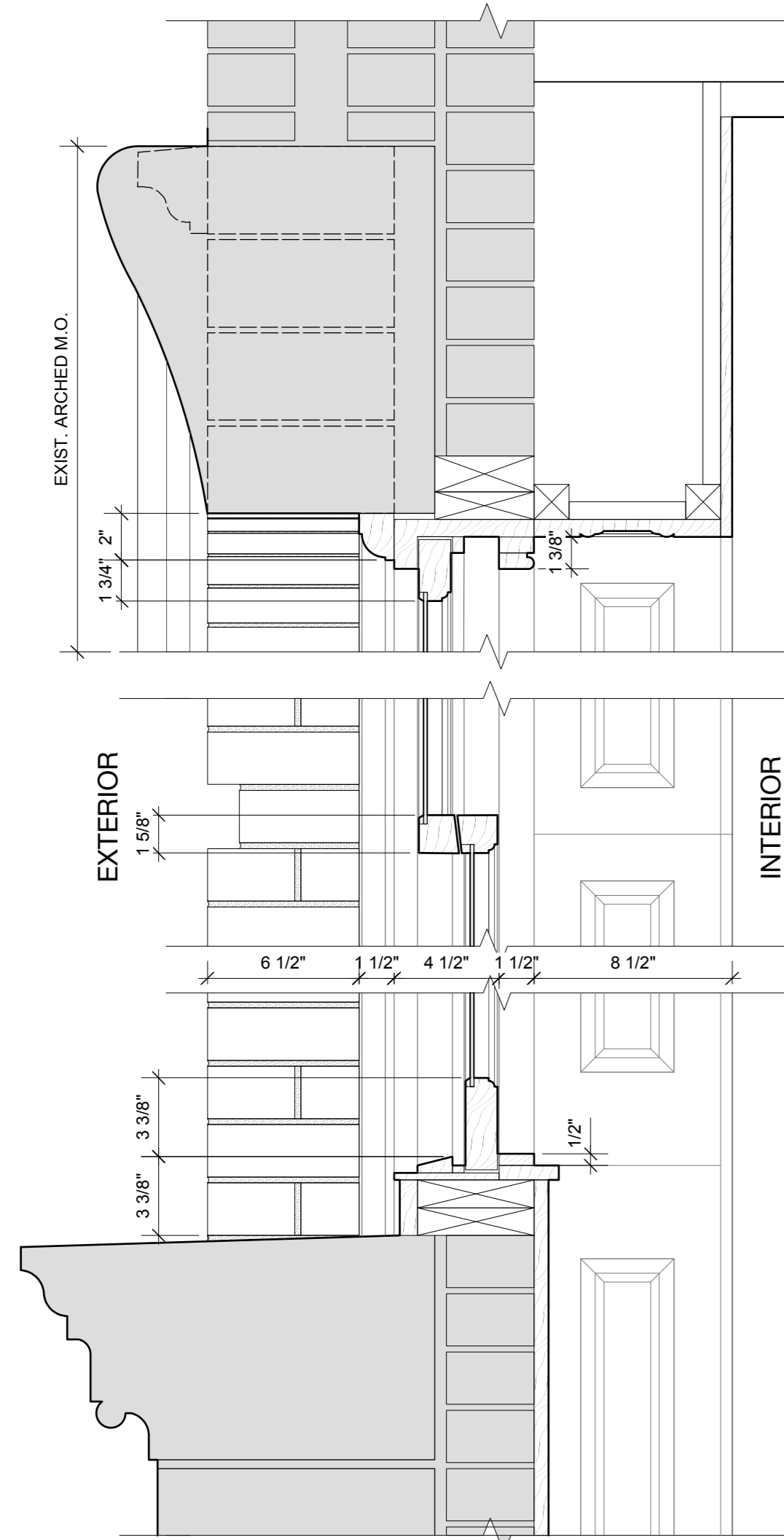
**SOUTH WINDOW DETAILS:
 FIRST FLOOR**

	REV#	ISSUE	DATE

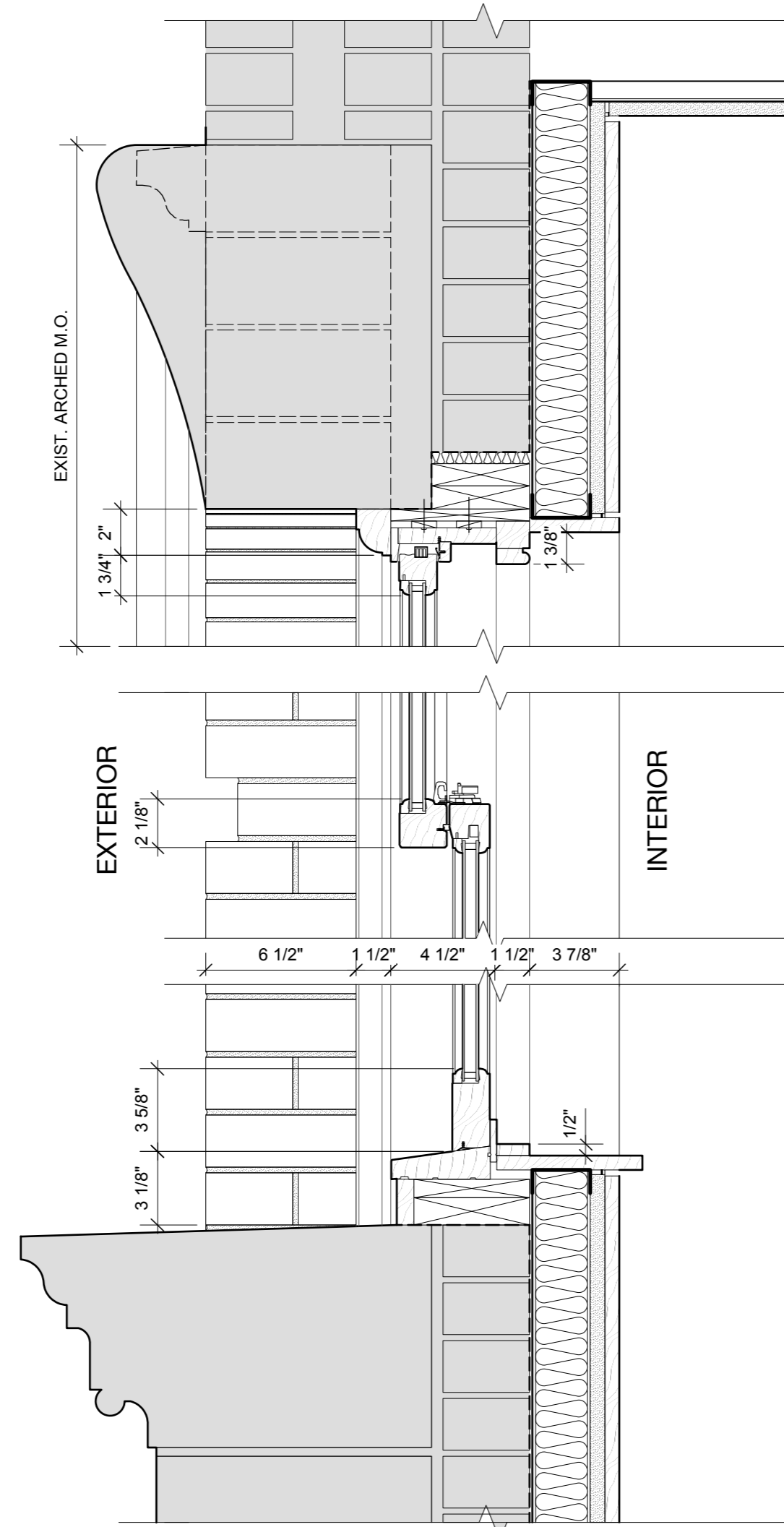
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NYC LPC SUBMISSION: 07. 10. 2015

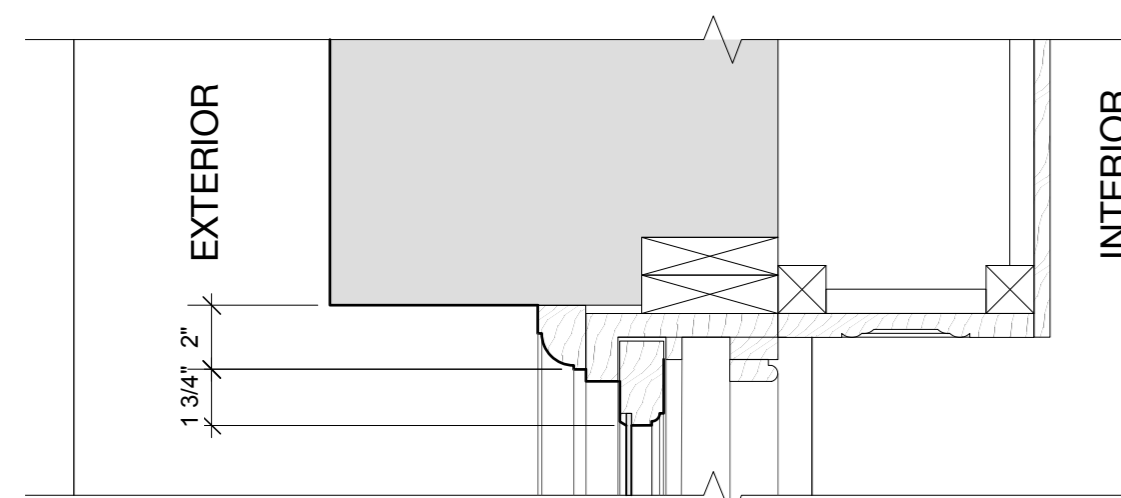
NYC DOB NUMBER	21.5K
Scale:	
24 OF 34	



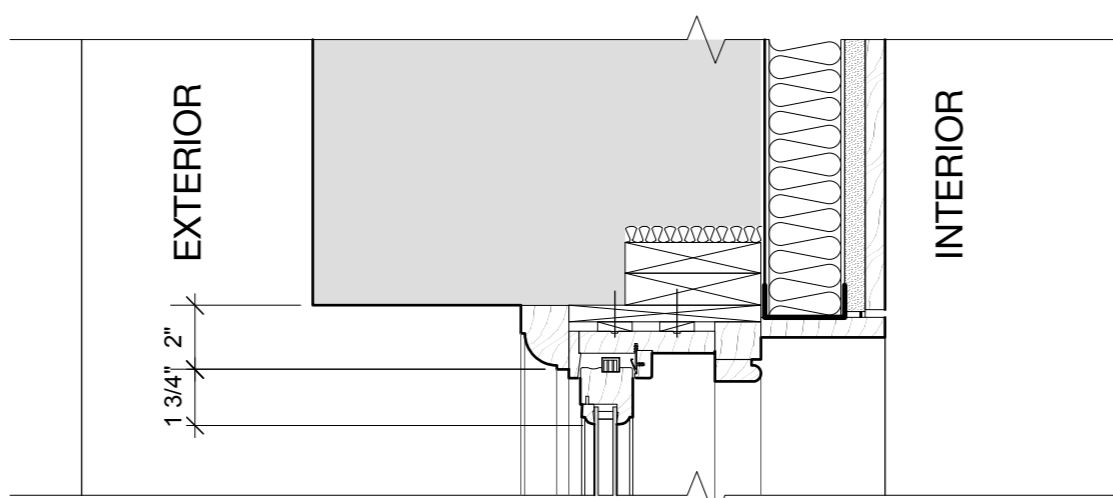
1 TYP. EXISTING WINDOW SECT. @ SECOND FLOOR
2" = 1'-0"



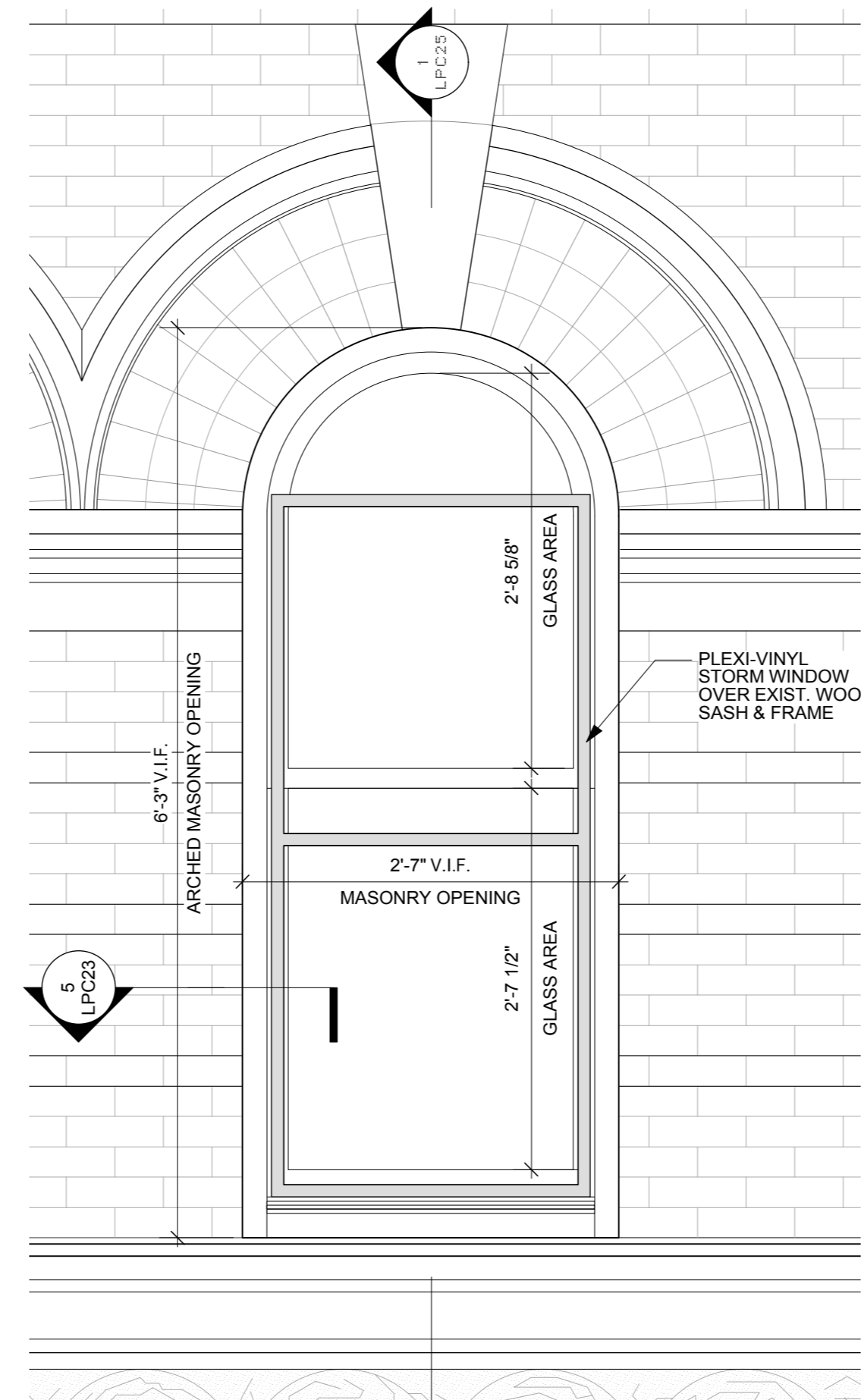
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2" = 1'-0"



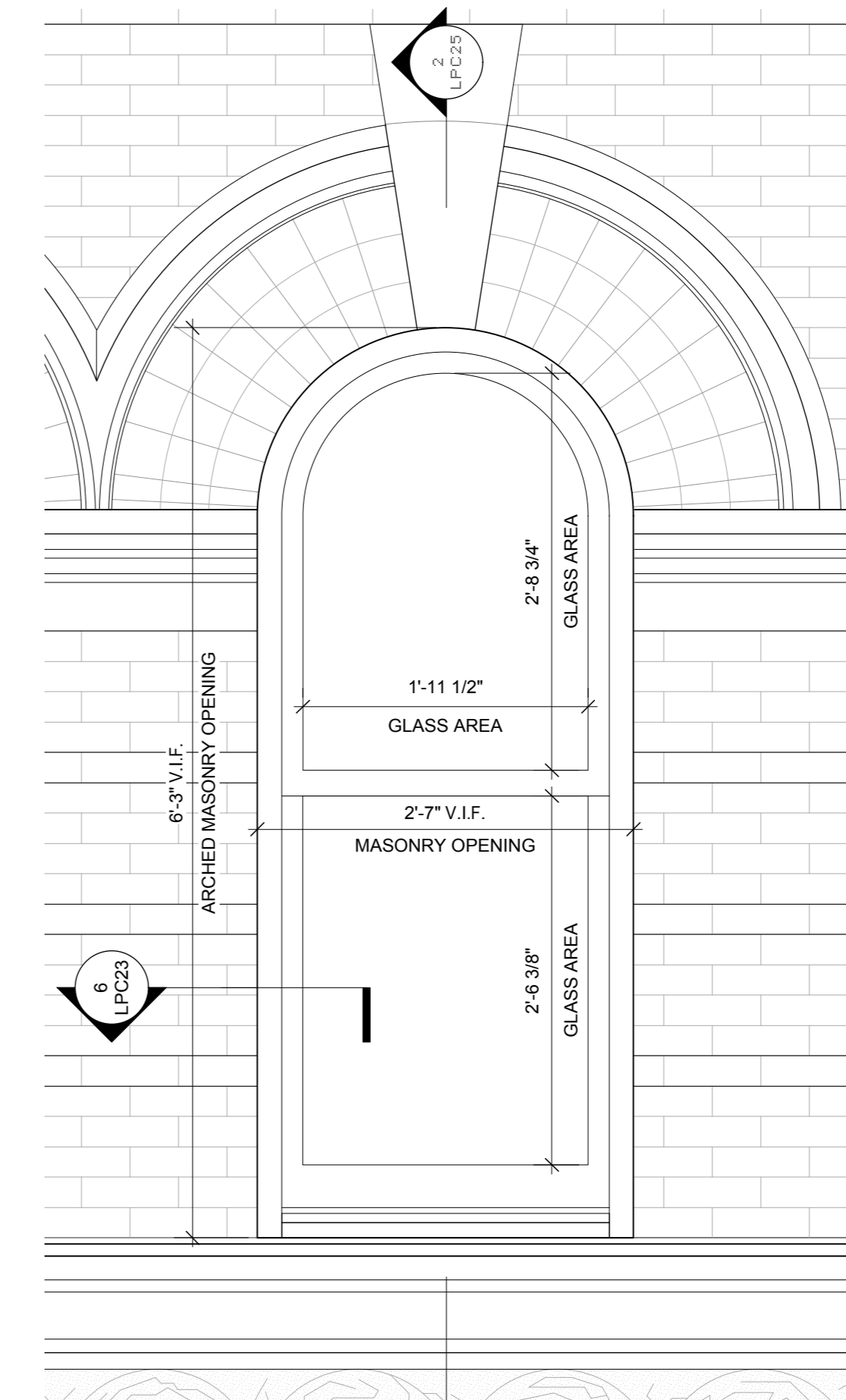
5 TYP. EXISTING WINDOW JAMB @ SECOND FLOOR
2" = 1'-0"



6 TYP. PROPOSED WINDOW JAMB @ SECOND FLOOR
2" = 1'-0"



3 EXISTING ELEVATION: SECOND FLOOR WINDOW
1" = 1'-0"



4 PROPOSED ELEVATION: SECOND FLOOR WINDOW
1" = 1'-0"

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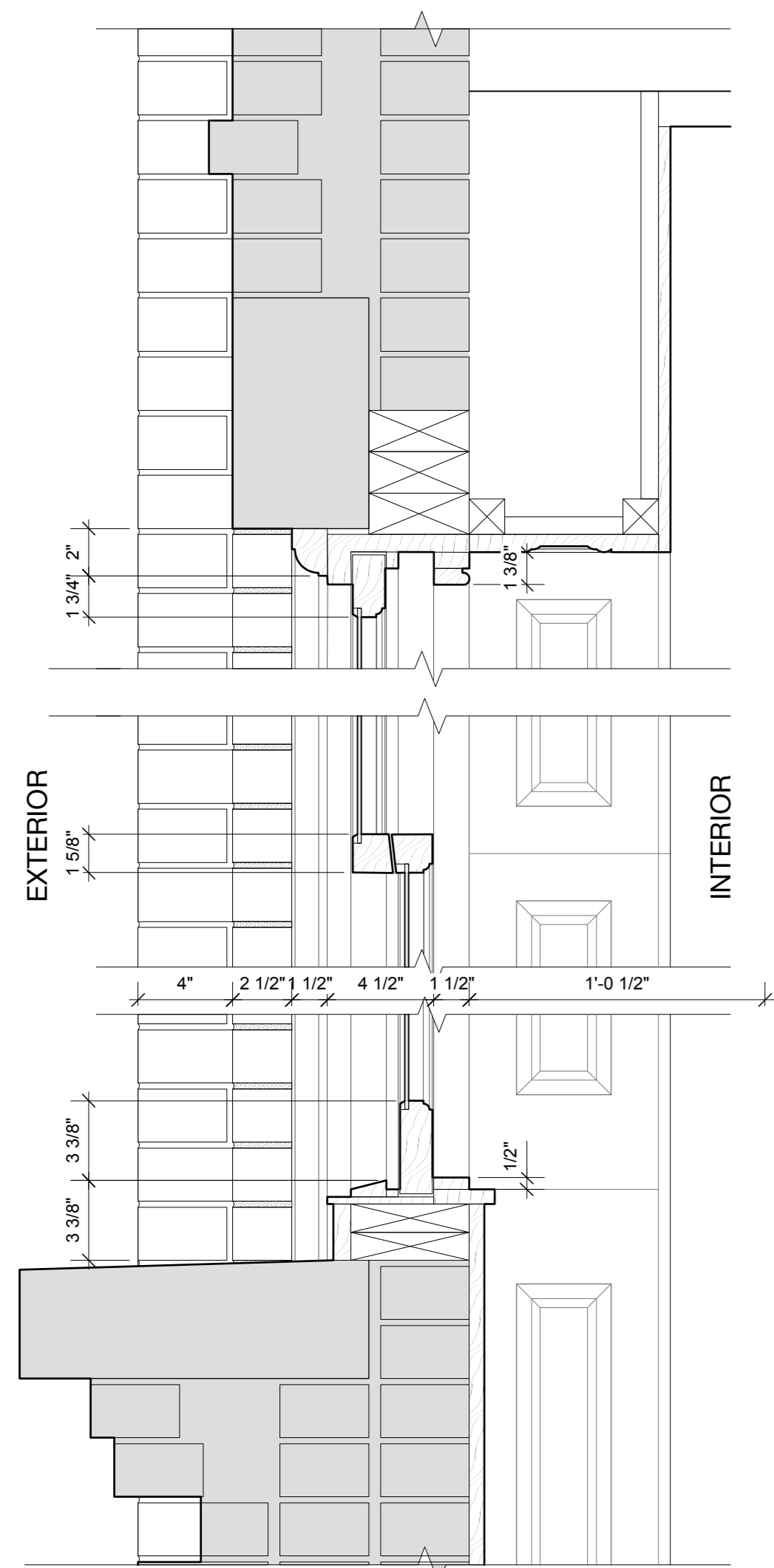
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SECOND FLOOR

	REV#	ISSUE	DATE

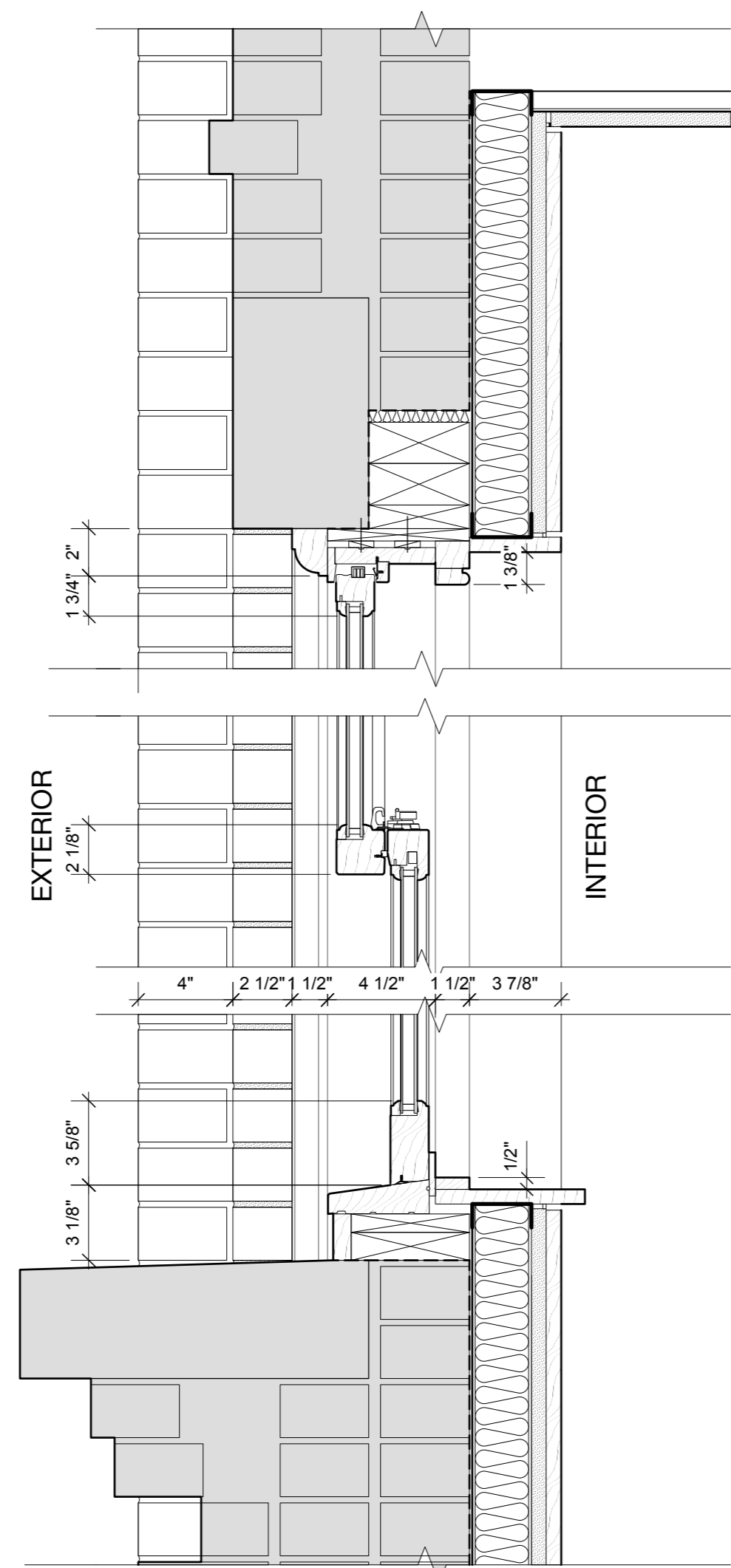
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NYC LPC SUBMISSION: 07. 10. 2015

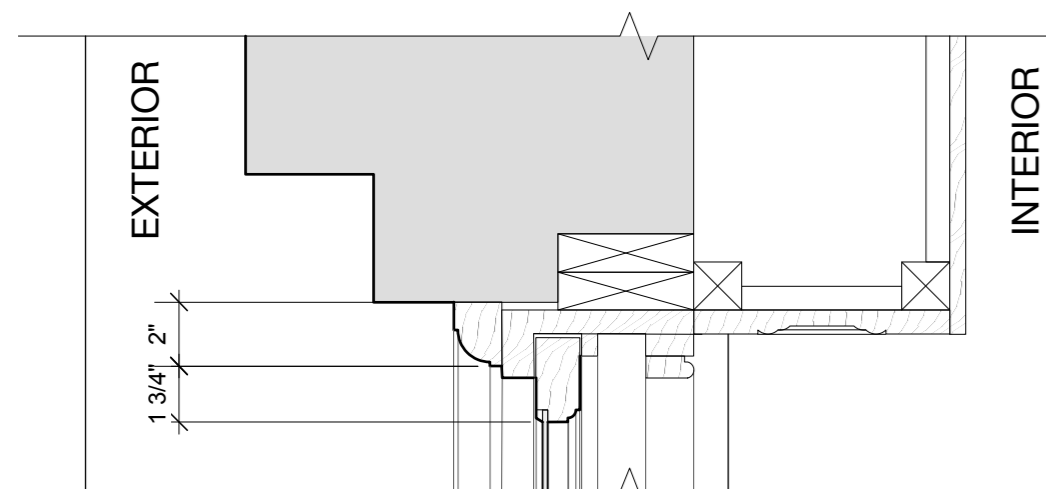
NYC DOB NUMBER	21.5K
Scale:	25 OF 34



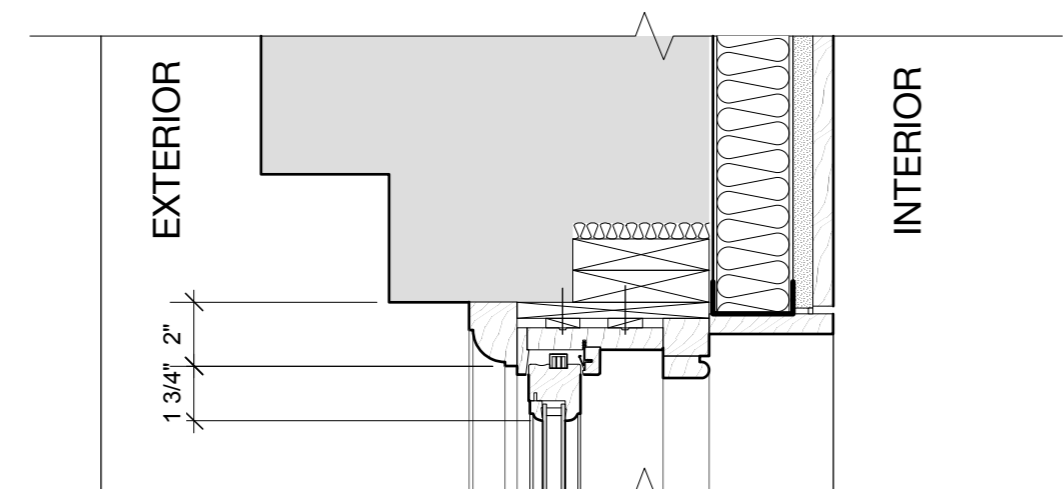
1 TYP. EXISTING WINDOW SECT. @ THIRD FLOOR
2" = 1'-0"



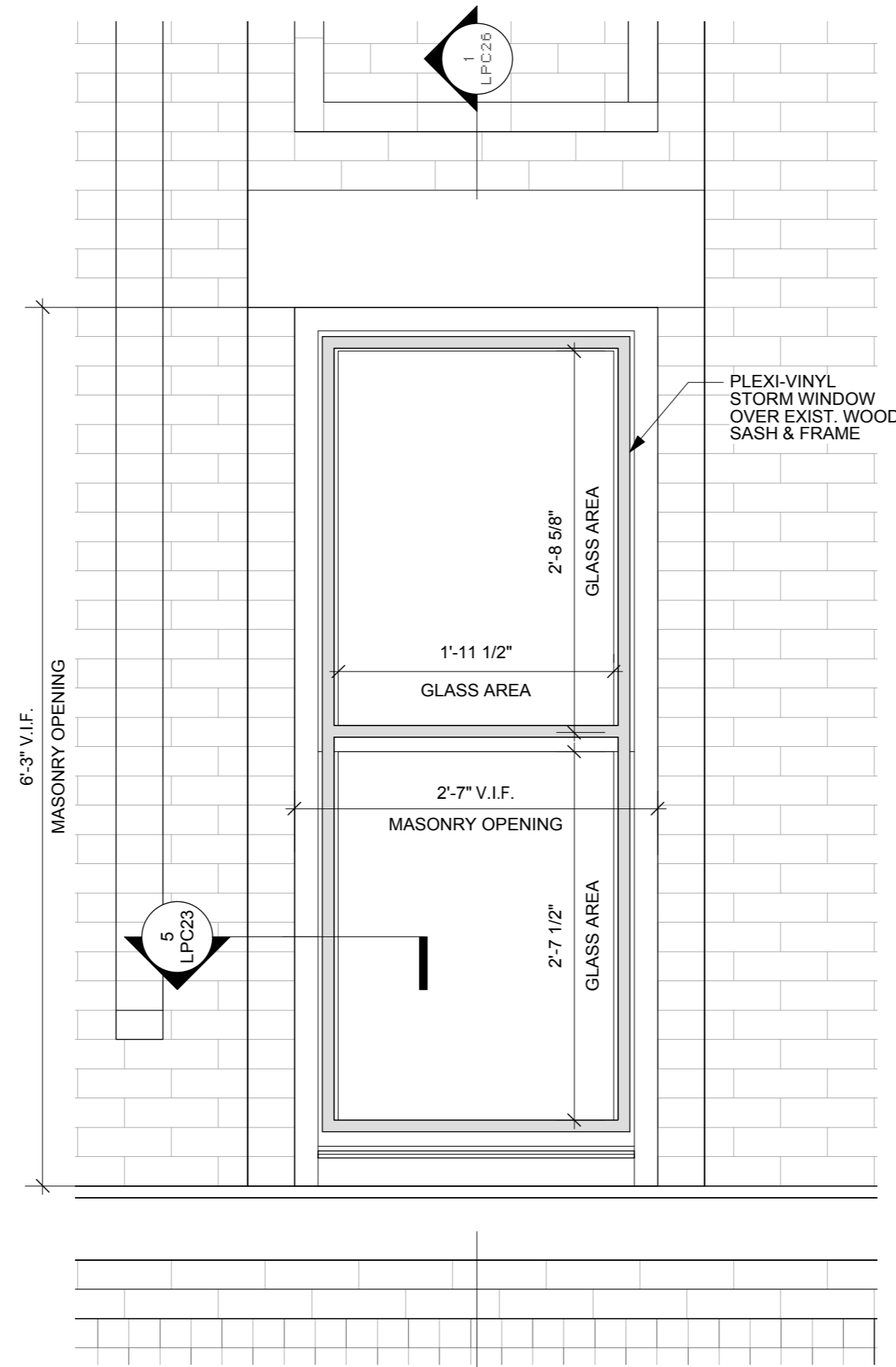
2 TYP. PROPOSED WINDOW SECT. @ THIRD FLOOR
2" = 1'-0"



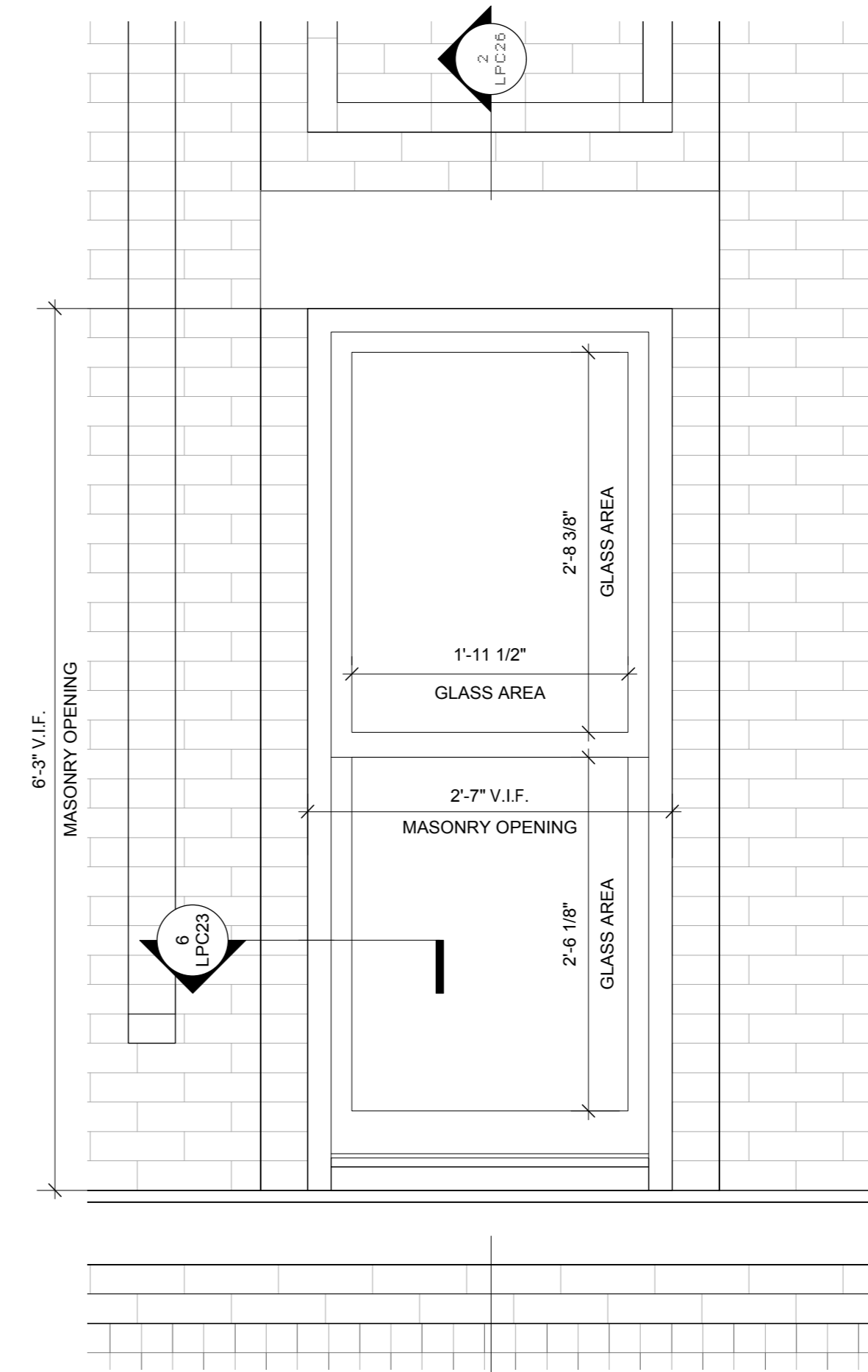
5 TYP. EXISTING WINDOW JAMB @ THIRD FLOOR
2" = 1'-0"



6 TYP. PROPOSED WINDOW JAMB @ THIRD FLOOR
2" = 1'-0"



3 EXISTING ELEVATION: THIRD FLOOR WINDOW
1" = 1'-0"



4 PROPOSED ELEVATION: THIRD FLOOR WINDOW
1" = 1'-0"

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SOUTH WINDOW DETAILS:
THIRD FLOOR

	REV#	ISSUE	DATE

LPC26.00

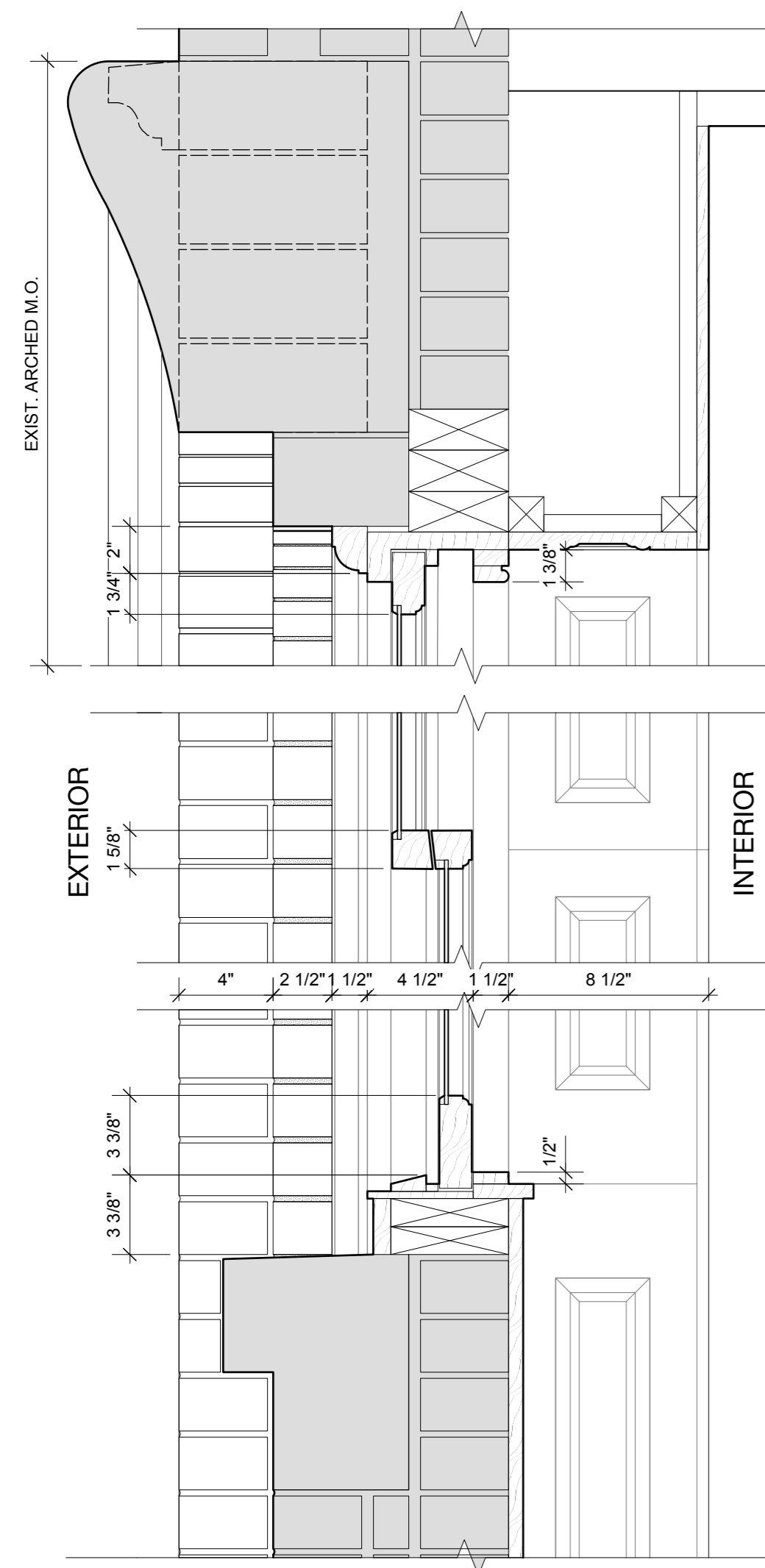
NYC LPC SUBMISSION: 07. 10. 2015

NYC DOB NUMBER

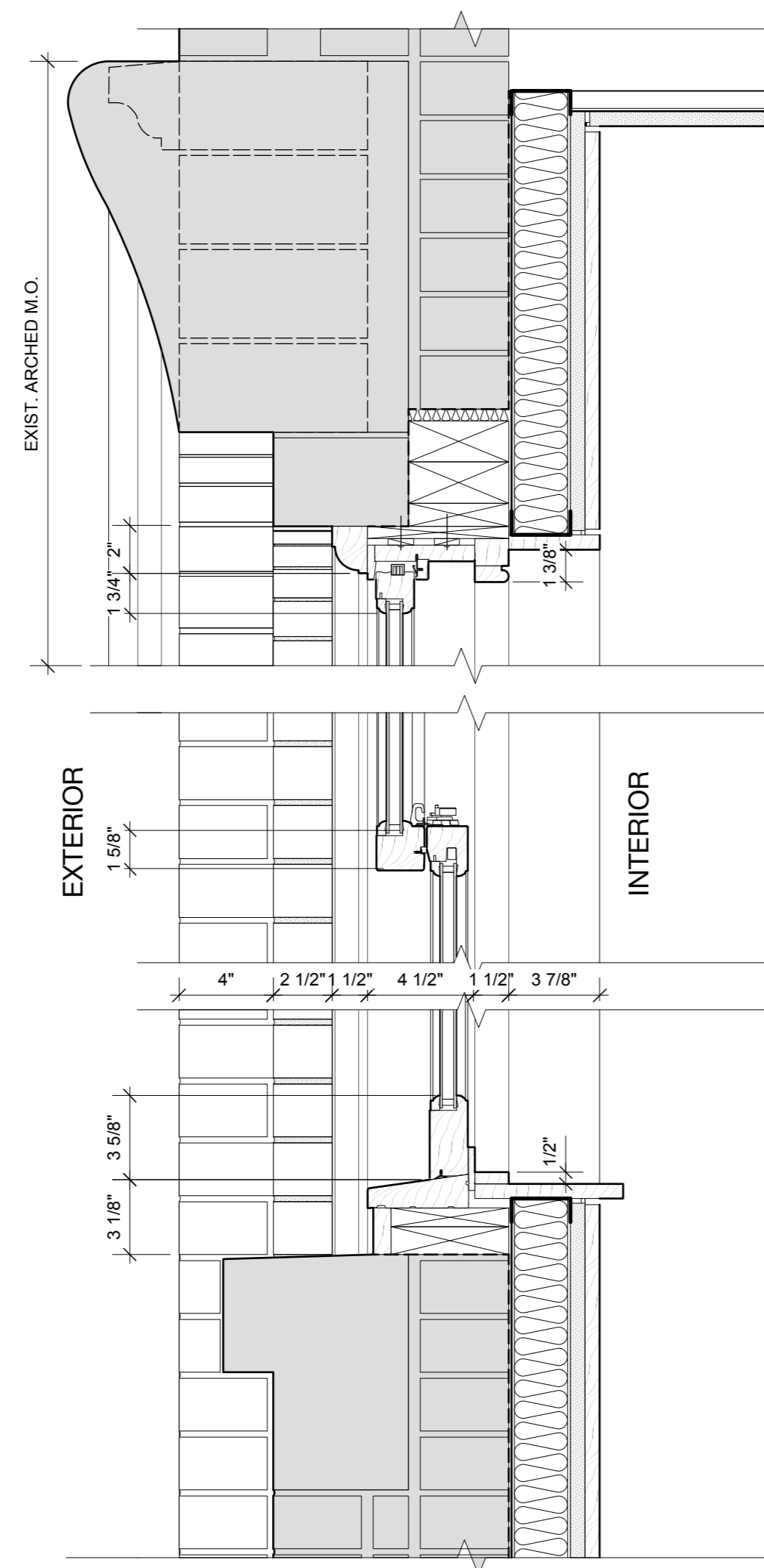
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Scale:

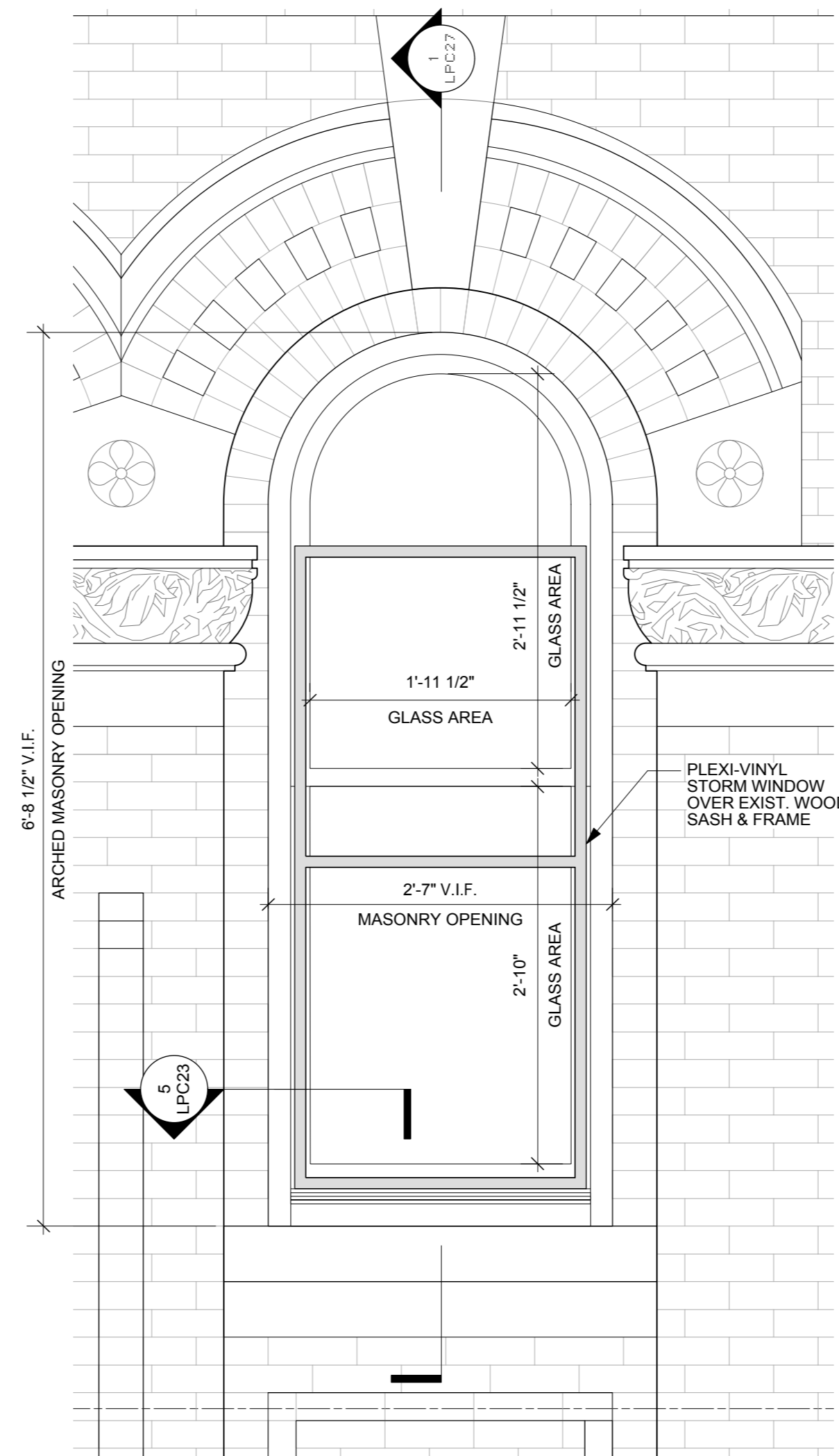
26 OF 34



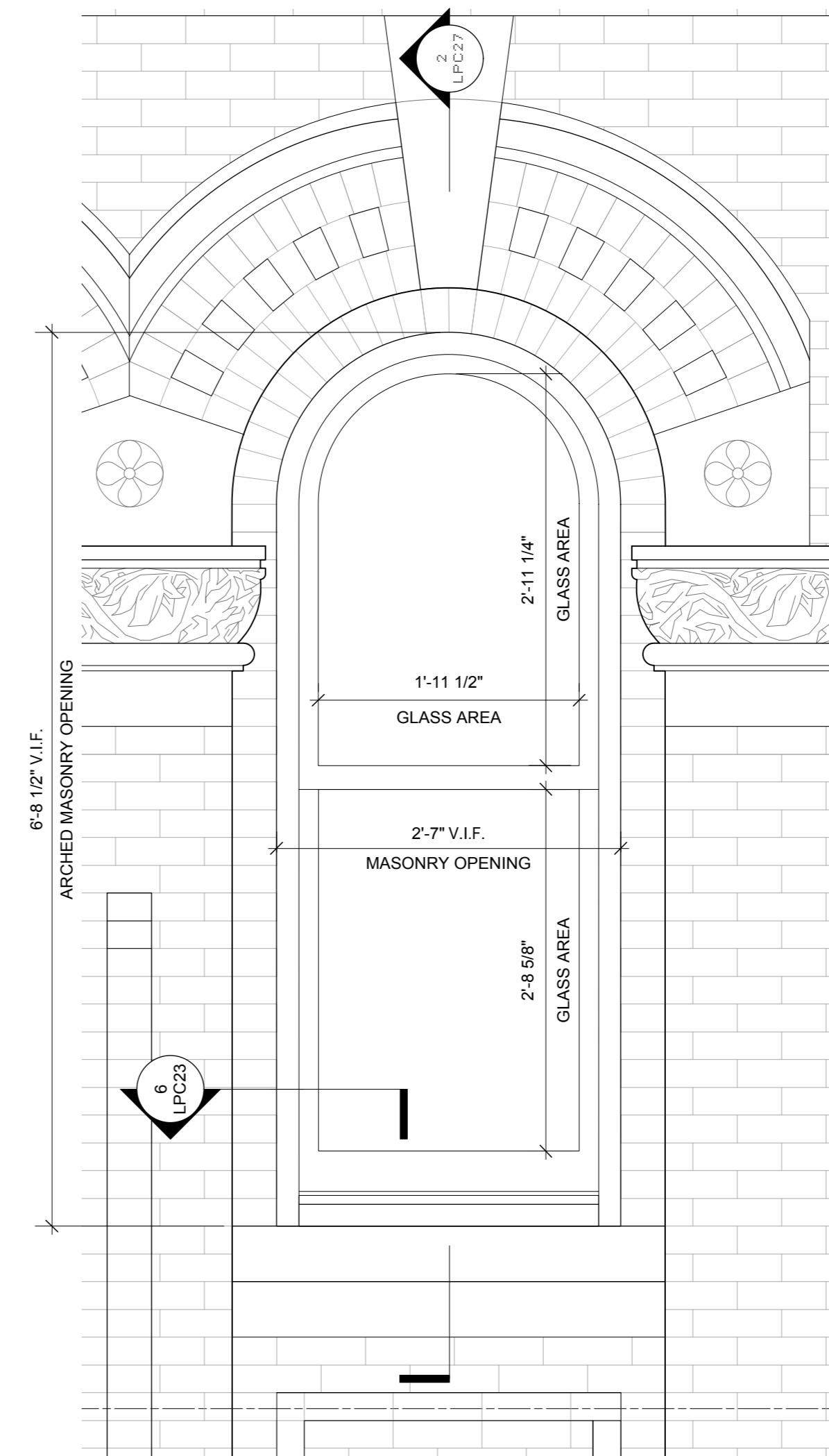
1 TYP. EXISTING WINDOW SECT. @ FOURTH FLOOR
2" = 1'-0"



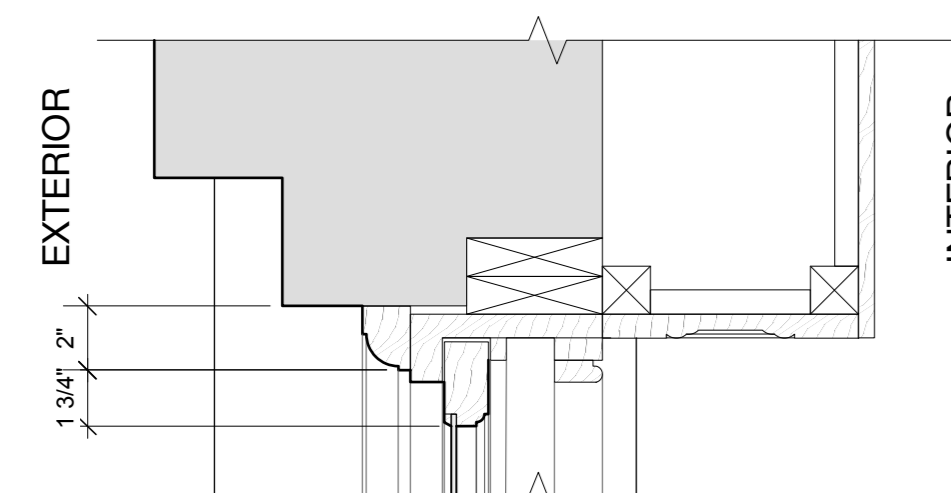
2 TYP. PROPOSED WINDOW SECT. @ FOURTH FLOOR
2" = 1'-0"



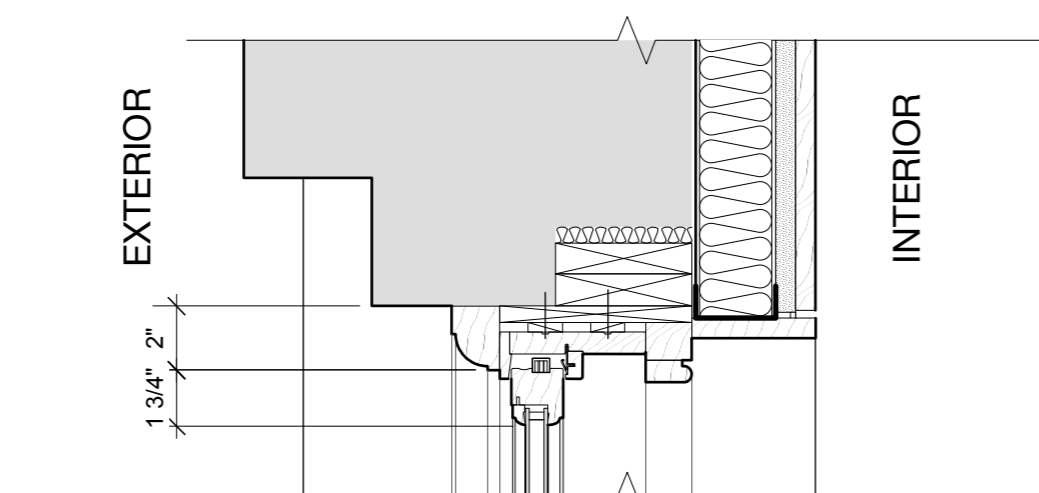
3 EXISTING ELEVATION: FOURTH FLOOR WINDOW
1" = 1'-0"



4 PROPOSED ELEVATION: FOURTH FLOOR WINDOW
1" = 1'-0"



5 TYP. EXISTING WINDOW JAMB @ FOURTH FLOOR
2" = 1'-0"



6 TYP. PROPOSED WINDOW JAMB @ FOURTH FLOOR
2" = 1'-0"

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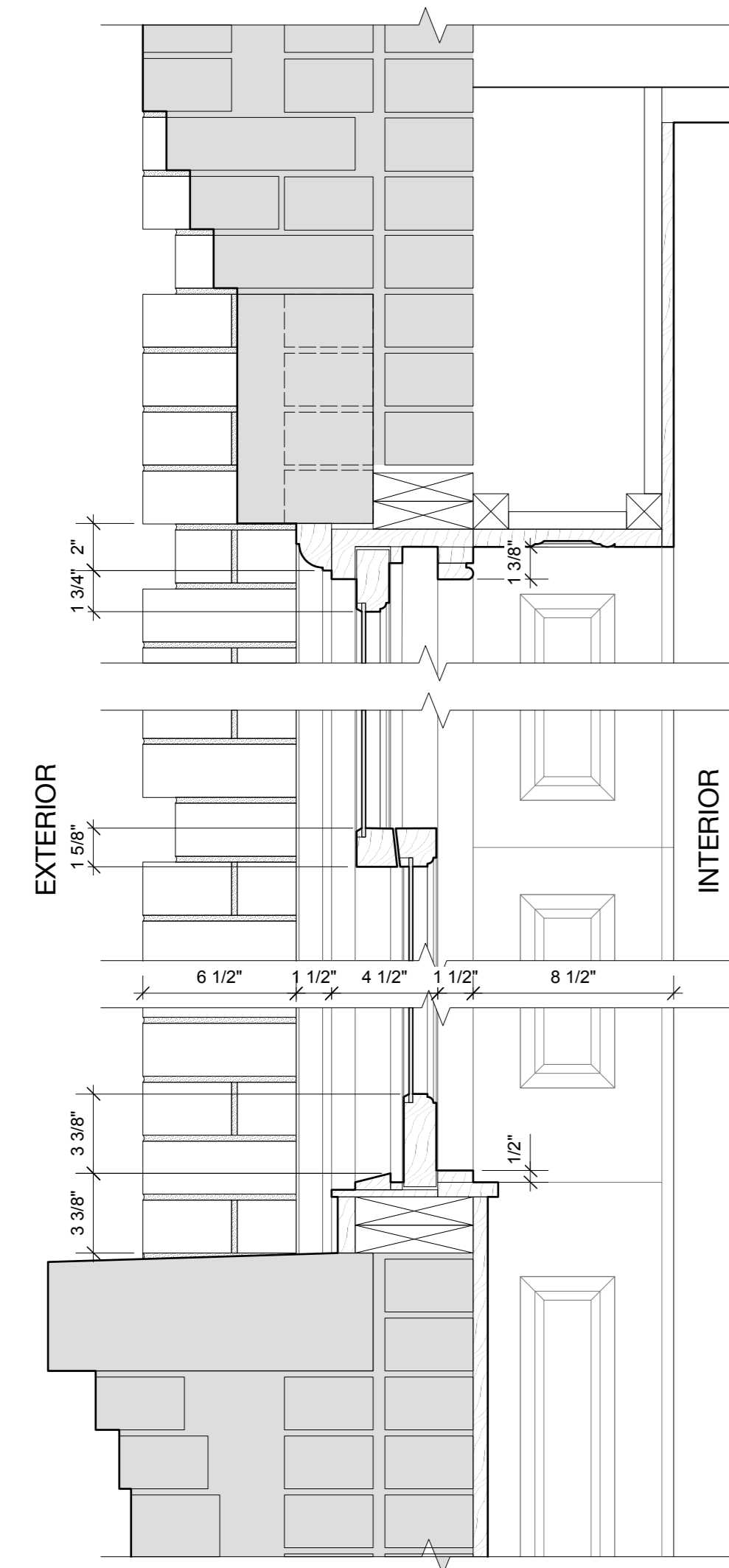
SOUTH WINDOW DETAILS:
FOURTH FLOOR

	REV#	ISSUE	DATE

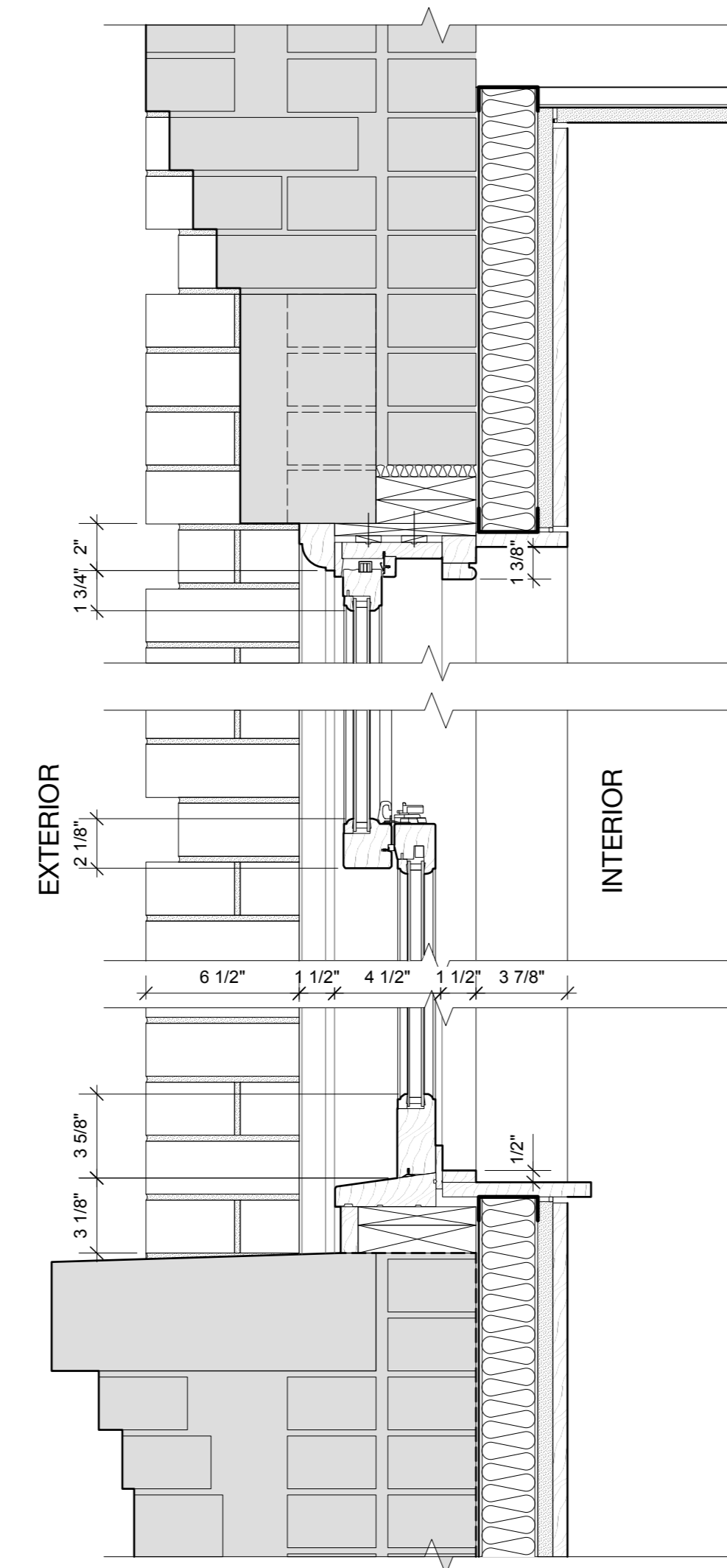
LPC27.00

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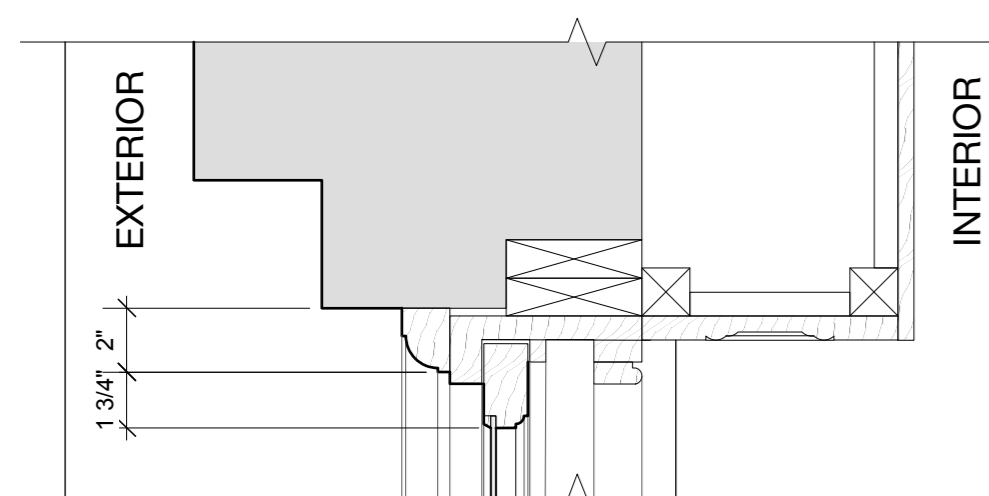
NYC DOB NUMBER	21.5K
Scale:	27 OF 34



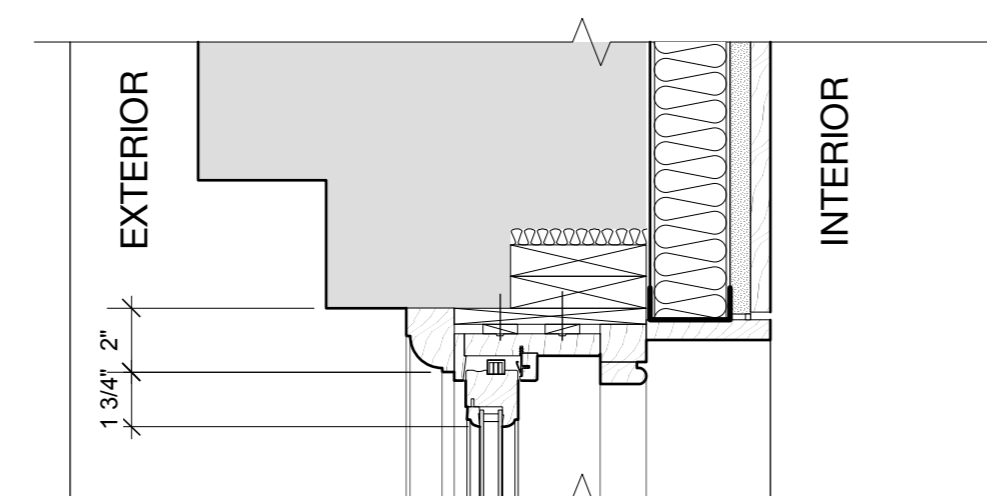
1 TYP. EXISTING WINDOW SECT. @ FIFTH FLOOR
2" = 1'-0"



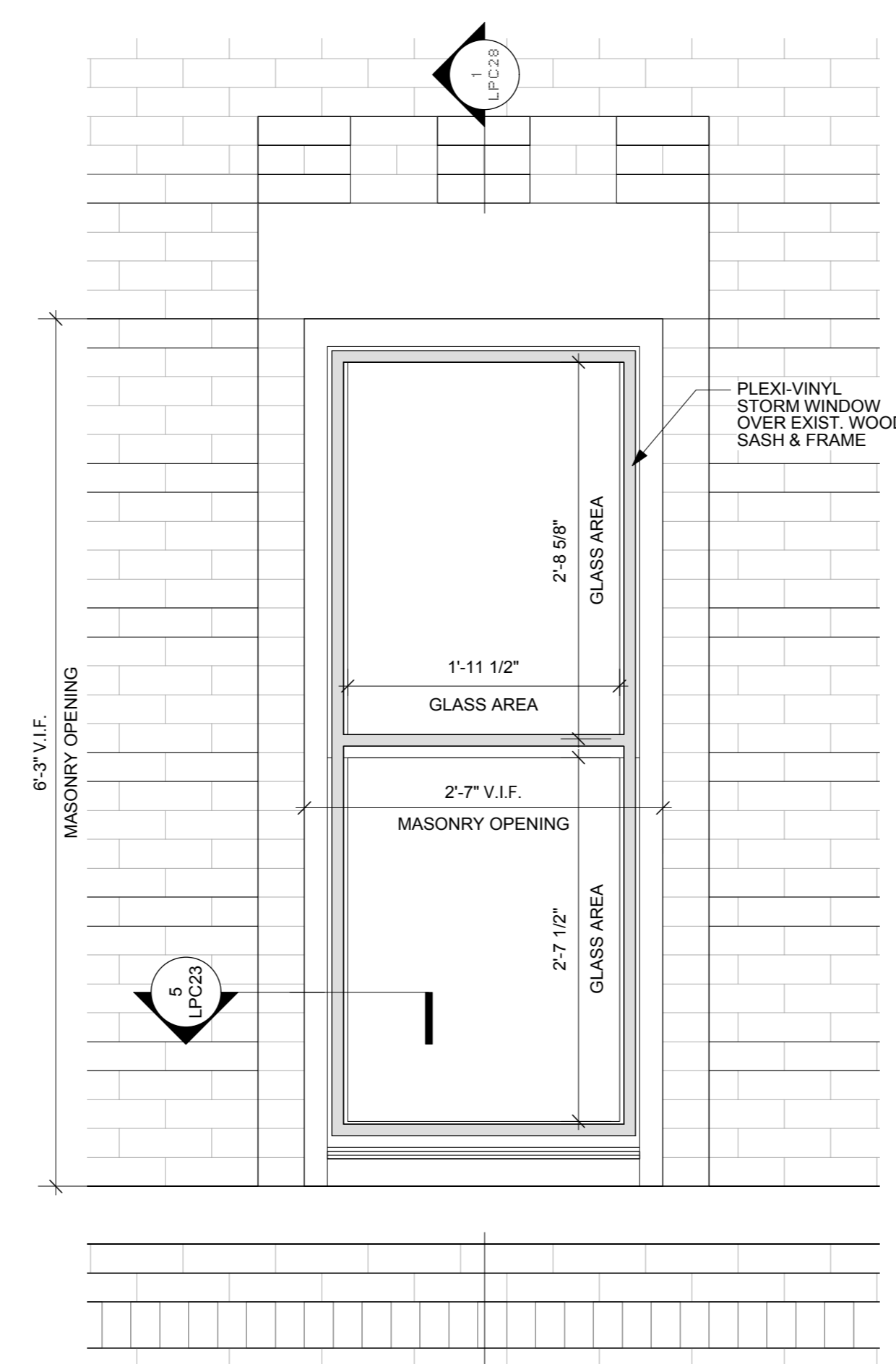
2 TYP. PROPOSED WINDOW SECT. @ FIFTH FLOOR
2" = 1'-0"



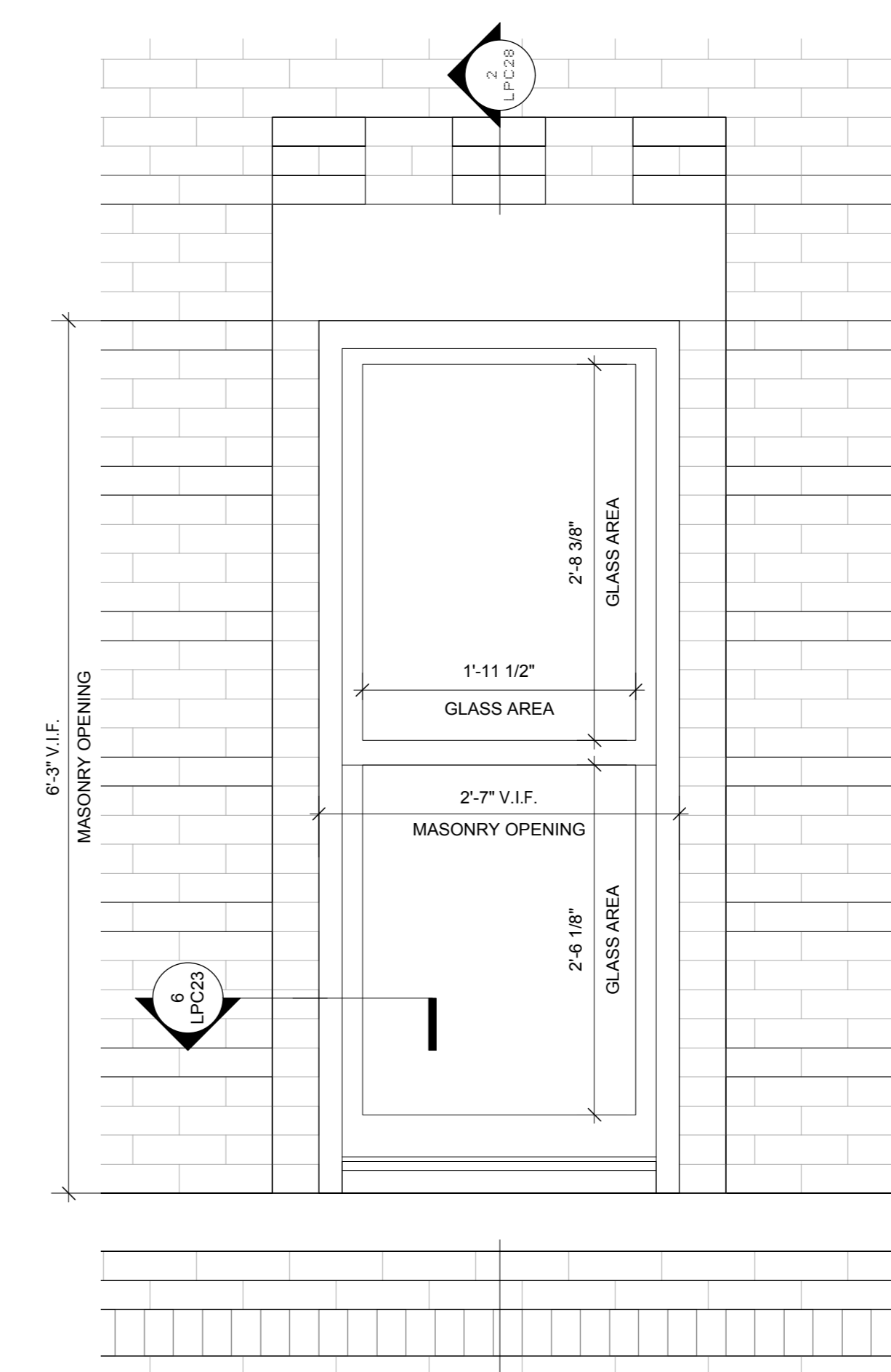
5 TYP. EXISTING WINDOW JAMB @ FIFTH FLOOR
2" = 1'-0"



6 TYP. PROPOSED WINDOW JAMB @ FIFTH FLOOR
2" = 1'-0"



3 EXISTING ELEVATION: FIFTH FLOOR WINDOW
1" = 1'-0"



4 PROPOSED ELEVATION: FIFTH FLOOR WINDOW
1" = 1'-0"

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SOUTH WINDOW DETAILS:
FIFTH FLOOR

○	REV#	ISSUE	DATE

LPC28.00

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HISTORIC RESTORATION NOTES:

EXTERIOR WOOD RESTORATION

1. TAKE ALL PRECAUTIONS AGAINST LEAD DUST, EYE DAMAGE AND ACCIDENTAL INHALATION OF PAINT DUST PARTICLES. DISPOSE OF LEAD PAINT RESIDUE PROPERLY AS REGULATED BY THE DEPTS. OF BUILDINGS AND SANITATION IN THE CITY OF NEW YORK.
2. REMOVE ALL LOOSE OR SCALING PAINT BY SCRAPING WITH ABRASIVE HAND TOOLS SUCH AS PUTTY KNIVES, PAINT SCRAPERS, SANDPAPER, SANDING BLOCKS AND SANDING SPONGES. ALL SANDING WORK SHOULD BE PERFORMED IN THE DIRECTION OF THE WOOD GRAIN. LIMITED USE OF ORBITAL AND BELT SANDERS ARE PERMITTED FOR 'FEATHERING' AREAS WHERE PAINT HAS BEEN HAND SCRAPPED. ABRASIVE MECHANICAL REMOVAL METHODS OF PAINT REMOVAL SUCH AS ROTARY DRILL ATTACHMENTS, WATER BLASTING (ABOVE 600 PSI) AND SANDBLASTING ARE NOT PERMITTED.
3. THERMAL REMOVAL METHODS SUCH AS ELECTRIC HEAT GUNS ARE PERMITTED. CHEMICAL REMOVAL METHODS SUCH AS SOLVENT-BASE AND CAUSTIC STRIPPERS ARE PERMITTED. TAKE ALL REQUIRED AND APPROPRIATE SAFE MEASURES TO PREVENT FIRE, DAMAGE OR INJURY WHEN USING THESE METHODS.
4. FILL ALL NICKS, GOUGES & CRACKS IN THE WOOD SURFACE WITH A WOOD PUTTY COMPOUND.
5. SAND THE ENTIRE WOOD SURFACE TO SMOOTH OUT WOOD PUTTY REPAIRS. SEE APPROVED SANDING METHODS ABOVE.
6. APPLY AN APPROVED OIL-BASED PRIMER BY BENJAMIN MOORE TO ALL EXPOSED WOOD SURFACES AND WOOD PUTTY REPAIRS. ENSURE THAT THE WOOD SURFACES ARE COMPLETELY DRY PRIOR TO APPLICATION OF PRIMER. MINIMUM 1-COAT APPLICATION OR AS RECOMMENDED BY PAINT MANUFACTURER.
7. APPLY AN APPROVED OIL-BASED PAINT BY BENJAMIN MOORE TO ALL PRIMED WOOD SURFACES. MINIMUM 2-COAT APPLICATION OR AS RECOMMENDED BY PAINT MANUFACTURER.
8. SPECIFIED PAINT COLOR: BENJAMIN MOORE # HC-70 VAN BUREN BROWN

EXTERIOR MASONRY RESTORATION & CLEANING

PROTECTION & PREPARATION:

- ALL MASONRY CLEANING, REPOINTING & PAINT REMOVAL WORK SHALL ONLY OCCUR WHEN THE AMBIENT OUTSIDE TEMPERATURE IS 45 DEGREES FAHRENHEIT AND ABOVE FOR A 72-HOUR PERIOD FROM THE COMMENCEMENT OF WORK. WORKING IN AMBIENT TEMPERATURES BELOW 45 DEGREES WILL NOT ALLOW FOR PROPER CURING, DRYING OR BONDING OF THE MATERIALS.
- TAKE ALL PRECAUTIONS AGAINST LEAD DUST, EYE DAMAGE AND ACCIDENTAL INHALATION OF PAINT DUST PARTICLES. DISPOSE OF LEAD PAINT RESIDUE PROPERLY AS REGULATED BY THE DEPTS. OF BUILDINGS AND SANITATION IN THE CITY OF NEW YORK.
- PROTECT ALL EXISTING WINDOWS, DOORS, FIXTURES AND OTHER BUILDING SURFACES FROM DAMAGE PRIOR TO APPLICATION OF EACH PRODUCT/ METHOD.
- FOR ADEQUATE SURFACE PREPARATION, REMOVE ANY VEGETATION, NESTS, ANIMAL DROPPINGS, MINERAL DEPOSITS AND HEAVY ACCUMULATION OF DIRT ON THE EXTERIOR FAÇADE PRIOR TO APPLICATION PAINT REMOVAL OR MASONRY CLEANING.
- PROVIDE MOCK-UPS OF MASONRY CLEANING & PAINT REMOVAL APPLICATION AT EACH TYPE OF EXTERIOR MASONRY MATERIAL. APPLY IN DISCRETE, LIMITED AREA FOR AOR/ LPC REVIEW BEFORE PROCEEDING.

REMOVE ANY EXISTING PAINT OR MASONRY COATINGS TO THE BRICK:

1. APPLY A CHEMICAL PAINT STRIPPER PRODUCT APPROVED FOR USE ON HISTORIC STRUCTURES. APPLY AS PER PRODUCT INSTRUCTIONS AND ALLOW TO SOAK IN FOR THE MANUFACTURER SPECIFIED PERIOD OF TIME.
 - HISTORIC PAINT STRIPPER:
SURE-AWAY # 7 BY DIJMOND CHEMICALS, INC. (OR EQUAL)
2. REMOVE ALL LOOSE OR SCALING PAINT BY SCRAPING WITH ABRASIVE HAND TOOLS. ABRASIVE MECHANICAL TOOLS ARE NOT PERMITTED.
3. A LOW-PRESSURE WATER WASH (BELOW 500 PSI) MAY ALSO BE USED TO REMOVE EXISTING PAINT. ABRASIVE & HIGH-PRESSURE WASHES ARE NOT PERMITTED.
4. REPEAT THE PROCESS AS NECESSARY TO ACHIEVE THE DESIRED LEVEL OF CLEANLINESS.
5. FOR ASPHALT OR TAR-COATED BRICK: APPLY CHEMICAL ASPHALT & TAR REMOVING PRODUCT APPROVED FOR USE ON HISTORIC STRUCTURES. APPLY AS PER PRODUCT INSTRUCTIONS AND ALLOW PRODUCT TO SOAK-IN/ SET FOR THE MANUFACTURER SPECIFIED PERIOD OF TIME. THE PRODUCT MAY BE APPLIED AND REMOVED USING A LOW-PRESSURE SPRAY (BELOW 50 PSI).
 - HISTORIC ASPHALT & TAR REMOVER:
SURE-KLEAN ASPHALT & TAR REMOVER BY PROSOCO, INC. (OR EQUAL)

CLEAN ALL EXISTING STONE & BRICK MASONRY:

1. APPLY A GENTLE MASONRY CLEANING DETERGENT PRODUCT APPROVED FOR USE ON HISTORIC STRUCTURES. CONSULT MANUFACTURER REGARDING ADDITIONAL PRE-WASH, AFTER-WASH & SPECIAL STAIN REMOVING PRODUCTS AS APPLICABLE/ RECOMMENDED.
 - HISTORIC MASONRY CLEANER FOR BRICK & SANDSTONE:
SURE-KLEAN RESTORATION CLEANER BY PROSOCO, INC. (OR EQUAL)
 - HISTORIC MASONRY CLEANER FOR LIMESTONE:
SURE-KLEAN LIMESTONE RESTORER BY PROSOCO, INC. (OR EQUAL)
2. APPLY A SOFT BRISTLE (NO METAL) BRUSH OR OTHER HAND TOOLS AS RECOMMENDED BY CLEANING DETERGENT MANUFACTURER. ABRASIVE MECHANICAL TOOLS ARE NOT PERMITTED.
3. A LOW-PRESSURE WATER WASH (BELOW 250 PSI) MAY ALSO BE USED TO REMOVE EACH CLEANING DETERGENT PRODUCT. ABRASIVE & HIGH-PRESSURE WASHES ARE NOT PERMITTED.
4. REPEAT THE PROCESS AS NECESSARY TO ACHIEVE THE DESIRED LEVEL OF CLEANLINESS.

RAKING & REPOINTING:

- ALL MASONRY WORK, TOOLS & MATERIALS SHALL BE PRE-CONDITIONED AND PROTECTED FROM COLD & HOT WEATHER CONDITIONS PER BC 2104.3 OF THE 2008 NYC BUILDING CODE.
- WATER SHALL BE CLEAN, POTABLE AND FREE OF FOREIGN MATTER, CONFORMING TO BC 1903.4 OF THE 2008 NYC BUILDING CODE.
- MORTAR COLORING: USE ONLY DRY, PURE MINERAL PIGMENTS FROM NATURAL AND SYNTHETIC IRON OXIDES AND CHROMIUM OXIDES FOR MORTAR MIXES. MATERIAL SHALL CONFORM TO ASTM C270. COLORING SHALL NOT CONTAIN ALKALINE SALTS. NO LIQUID COLORANTS ARE PERMITTED.
- USE OF BATCHED MATERIAL BY SPEC-MIX AND FACTORY PACKAGED CEMENT-LIME-PIGMENT BY SIMILAR MORTAR MANUFACTURERS IS PERMITTED. PRE-MIXED SAND & LIME MORE MIXES ARE NOT ALLOWED.
- NO AIR-ENTRAINING ADMIXTURES OR MATERIAL CONTAINING SUCH IS PERMITTED IN THE MORTAR. NO ANTI-FREEZE OR CALCIUM CHLORIDE COMPOUNDS ARE PERMITTED IN THE MORTAR.
- MORTAR MIXTURE: ALL MORTAR SHALL CONFORM TO ASTM C270 AND BIA-M1-88
 - 1 PART WHITE PORTLAND CEMENT [ASTM C-150, TYPE II]
 - 2 1/2 PARTS LIME
 - 5-6 PARTS SAND
 - PARTS ARE BY VOLUME
 - MIX DRY INGREDIENTS FIRST BEFORE ADDING POTABLE WATER
 - USE DRY PIGMENTS
 - MIX ALL INGREDIENTS THOROUGHLY

HISTORIC RESTORATION NOTES (CONTINUED) :

RAKING & REPOINTING CONTINUED

1. RAKE OR CUT OUT ALL MASONRY JOINTS TO A MINIMUM DEPTH OF 3/4" AND UNTIL A SOUND SUB-SURFACE IS REACHED.
 - DO NOT SPALL EDGES OF MASONRY OR WIDEN EXISTING JOINTS.
 - ALL VERTICAL JOINTS & HORIZONTAL JOINTS 1/8" OR LESS SHALL BE RAKED WITH NON-POWER TOOLS.
 - ALL HORIZONTAL JOINTS THICKER THAN 1/8", A COMBINATION OF POWER TOOLS AND HAND HELD NON-POWER TOOLS MAY BE USED.
 - ALL POWER TOOLS SHALL HAVE DIAMOND OR CARBIDE TIP BLADES/ GRINDER BITS.
 - REMOVAL OF MORTAR SHALL BE PERFORMED WITHOUT DAMAGE TO THE EXISTING MASONRY. REPLACE ALL MASONRY DAMAGED BY SUCH OPERATIONS WITH NEW MASONRY TO MATCH IN COLOR, SIZE AND TEXTURE.
 - CUT MORTAR JOINTS CLEANLY FROM THE SIDES OF JOINTS, LEAVING SQUARE CORNERS. FLUSH OUT JOINTS CLEAN WITH WATER OR COMPRESSED AIR.
2. DAMPEN JOINTS SLIGHTLY BEFORE APPLICATION OF MORTAR. PACK POINTING MORTAR TIGHTLY IN THIN LAYERS OR LIFTS (1/4" MAXIMUM DEPTH). ALLOW EACH LAYER OR LIFT TO SET UNTIL "THUMBPRINT HARD" BEFORE APPLYING THE NEXT LAYER OR LIFT. A MINIMUM OF 3 LAYERS OR LIFTS IS REQUIRED FOR POINTING.
3. ALL NEW JOINTS SHALL TOOLED WITH A CONCAVE PROFILE OR MATCH EXISTING ADJACENT JOINT PROFILE. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS REQUIRED TO ENSURE THE ORIGINAL APPEARANCE OF THE BUILDING IS MAINTAINED/ RESTORED AND THAT THE EXISTING MASONRY IS NOT FURTHER DAMAGED. THE NEW MORTAR SHALL MATCH THE ORIGINAL MORTAR IN COLOR & TEXTURE.
4. WHERE JOINT SEALANT IS REQUIRED, TIGHTLY PACK THE BACK OF THE JOINT TO A UNIFORM DEPTH OF 1/4". INSTALL BACKER ROD OR COMPRESSIBLE FILL MATERIAL AS SPECIFIED IN THE DRAWINGS. APPLY BONDBREAKER PRIOR TO INSTALLING SEALANTS. APPLY EXTERIOR GRADE, NO-SAG SEALANT AS SPECIFIED IN THE DRAWINGS.
5. ALLOW REPOINTED MORTAR TO CURE BY MAINTAINING A DAMP CONDITION FOR AT LEAST 72 HOURS.

REPLACEMENT OF MASONRY UNITS

1. WHERE NEEDED OR BY DAMAGE CAUSED DURING CONSTRUCTION OPERATIONS, THE CONTRACTOR SHALL REPLACE SPALLED, CRACKED AND DAMAGED MASONRY TO MATCH EXISTING IN COLOR, SHAPE, SIZE AND TEXTURE. THE CONTRACTOR SHALL PROVIDE TEMPORARY SHORING OR OTHER SUPPORTS AS REQUIRED FOR STRUCTURAL STABILITY.
2. CUT THE PERIMETER JOINTS AROUND THE DAMAGED MASONRY UNIT. LEAVE SQUARE CORNERS AT ADJOINING MASONRY TO REMAIN. REMOVE THE DAMAGED AND DETERIORATED MASONRY UNITS TO THEIR FULL DEPTH INCLUDING THE SURROUNDING MORTAR.
3. DAMPEN ALL CONTACT SURFACES BEFORE APPLICATION OF MORTAR AND INSTALLED A NEW MATCHING MASONRY UNIT WITH TYPE N MORTAR. ALIGN NEW MASONRY TO EXISTING. MAINTAIN EXISTING BONDING & COURSING PATTERNS. USE PRE-SOAKED WOOD WEDGES WHERE NECESSARY TO PROPERLY SET NEW MASONRY UNITS IN PLACE.
4. TIGHTLY BACK-PACK & FILL JOINTS WITH MORTAR TO ENSURE THERE ARE NO VOIDS. FILL ALL OPEN JOINTS IN BACKUP MASONRY AND COLLAR JOINTS. FINISH ALL JOINTS TO WITH A CONCAVE PROFILE OR TO MATCH EXISTING ADJACENT JOINT PROFILE.

STONE MASONRY REPAIR & PATCHING (FOR LIMESTONE, SANDSTONE & BROWNSTONE)

1. SURFACE PREPARATION: REMOVE ALL CRACKED, SPALLED & DETERIORATED STONE MATERIAL DOWN TO A SOUND SURFACE AT LOCATIONS SHOWN ON THE DRAWINGS AND AS DIRECTED BY THE AOR. CUT BACK DAMAGED MATERIAL WITH A TOOTHED CHISEL AND REMOVE LOOSE STONE TO PROVIDE A ROUGH TEXTURED SUBSTRATE. PROVIDE PROTECTION OF ADJACENT UNDAMAGED STONE SURFACES DURING REMOVALS AND REPAIRS.
 2. CUT THE EDGES OF THE REPAIR AREA TO PROVIDE A MINIMUM DEPTH OF 1/4". THE EDGES OF THE REPAIR SHOULD BE SQUARE CUT. DO NOT ALLOW ANY FEATHERED EDGES IN THE REPAIR AREA. PRE-WET REPAIR AREA TO PREVENT EXCESSIVE MOISTURE WICKING FROM REPAIR MORTAR. RE-WET AREA PRIOR TO APPLICATION OF REPAIR MORTAR PRODUCT.
 3. APPLY A MASONRY RESTORATION MORTAR PRODUCT APPROVED FOR USE ON HISTORIC STRUCTURES. CONSULT MANUFACTURER REGARDING ADDITIONAL ASSOCIATED PRODUCTS FOR SURFACE PREPARATION & CLEANINGS AS APPLICABLE/ RECOMMENDED.
 - HISTORIC MASONRY RESTORATION (PATCHING) MORTAR:
JAHN M70 BY CATHEDRAL STONE PRODUCTS, INC. (OR EQUAL)
- APPLY A 1/8" THICK CONTINUOUS LAYER OF REPAIR MORTAR MIXED WITH WATER WITH THE CONSISTENCY OF WET PUTTY ("PEANUT BUTTER") AND PREVENT FROM DRYING OUT. TROWEL INTO PLACE A DRIER (5-1) MIX OF REPAIR MORTAR AND WATER TO THE AREA AND PACK FIRMLY TO ELIMINATE VOIDS. ALLOW TO CURE AS RECOMMENDED BY MANUFACTURER. APPLY MORTAR MIX IN MULTIPLE THIN LAYERS FOR REPAIRS GREATER THAN 2" IN DEPTH AND ALLOW TO CURE BETWEEN APPLICATIONS. SCRAPE OFF EXCESS MATERIAL AND TOOL THE REPAIR MORTAR SURFACE TO MATCH THE EXISTING ADJACENT STONE SURFACES.

BROWNSTONE (SANDSTONE) RESTORATION & RESURFACING

1. SURFACE PREPARATION: REMOVE ALL CRACKED, SPALLED & DETERIORATED SANDSTONE MATERIAL DOWN TO A SOUND SURFACE. CUT BACK DAMAGED MATERIAL WITH A TOOTHED CHISEL AND REMOVE LOOSE STONE TO PROVIDE A ROUGH TEXTURED SUBSTRATE.
2. KEYING SURFACE: UNDERCUT THE PERIMETER EDGES TO CREATE A DOVETAIL SHAPE AROUND THE AREA TO BE REPAIRED. DRILL 1/2" DIA. HOLES (A MINIMUM 1/2" DEPTH AND SPACED IN STAGGERED ROWS AT 2"-3" ON CENTER) TO CREATE A SURFACE FOR MECHANICAL BONDING BETWEEN EXISTING AND NEW MATERIALS. VARY THE ANGLE OF THE HOLES.
3. WASH THE PREPARED SURFACE WITH WATER AND A SOFT BRUSH.
4. APPLY A SLURRY COAT WITH A BRUSH AND WORK THOROUGHLY INTO THE SURFACE.

SLURRY COAT MIX (BY VOLUME)
1 PART WHITE PORTLAND CEMENT
2 PART TYPE S LIME
6 PARTS SAND
MIX WITH POTABLE WATER

5. APPLY SCRATCH COATS IN A SERIES OF LAYERS. THE FIRST SCRATCH COAT SHOULD BE APPLIED WHILE THE SLURRY COAT IS STILL MOIST. EACH SCRATCH COAT SHOULD BE SCORED BEFORE INITIAL DRYING TO PROVIDE A KEY FOR SUBSEQUENT COATS. EACH INDIVIDUAL SCRATCH COAT SHALL NOT EXCEED 3/8" IN THICKNESS.

SCRATCH COAT MIX (BY VOLUME)
1 PART WHITE PORTLAND CEMENT
1 PART TYPE S LIME
6 PARTS SAND
MIX WITH POTABLE WATER

6. APPLY A FINISH COAT AFTER THE SCRATCH COAT LAYERS HAVE BEEN BUILT UP TO THE NECESSARY THICKNESS. FORMULATE THIS FINAL COAT WITH DRY PIGMENTS (NATURAL OR SYNTHETIC STABLE OXIDE) AND CRUSHED STONE (16-MESH, WASHED) TO ACHIEVE A COLOR TO MATCH THE COLOR OF THE EXISTING ADJACENT BROWNSTONE FINISHES AT THE SISTER BUILDINGS AT 23 AND 25 KING STREET

FINISH COAT (BY VOLUME)
1 PART WHITE PORTLAND CEMENT
1 PART TYPE S LIME
2 - 3 PARTS SAND
3 - 4 PARTS CRUSHED STONE
DRY PIGMENTS
MIX WITH POTABLE WATER

7. FINISH THE SURFACE TO MATCH THE EXISTING/ ORIGINAL STONE SURFACE TEXTURE. SURFACE TREATMENTS MAY INCLUDE DAMP SPONGING (STIPLING), DRY TROWELING WITH WOODEN FLOAT AND/OR ACID ETCHING (WITH DILUTED HYDROFLUORIC ACID). EXECUTE ALL SURFACE FINISHING WHILE THE MATERIAL IS PARTIALLY CURED (TO LEATHER-LIKE HARDNESS).

HISTORIC RESTORATION NOTES (CONTINUED) :

IRON WORK RESTORATION

1. TAKE ALL PRECAUTIONS AGAINST LEAD DUST, EYE DAMAGE AND ACCIDENTAL INHALATION OF PAINT DUST PARTICLES. DISPOSE OF LEAD PAINT RESIDUE PROPERLY AS REGULATED BY THE DEPTS. OF BUILDINGS AND SANITATION IN THE CITY OF NEW YORK.
2. REMOVE ALL LOOSE OR SCALING PAINT BY SCRAPING WITH ABRASIVE HAND TOOLS SUCH AS PAINT SCRAPERS, WIRE BRUSHES, SYNTHETIC STEEL WOOL AND ABRASIVE PADS. LIMITED USE OF ORBITAL AND BELT SANDERS ARE PERMITTED FOR 'FEATHERING' AREAS WHERE PAINT HAS BEEN HAND SCRAPPED. LOW-PRESSURE WASH IS PERMITTED. ON-SITE SANDBLASTING IS NOT PERMITTED.
3. WIPE DOWN ENTIRE METAL SURFACE WITH MINERAL SPIRITS TO REMOVE ANY REMAINING MOISTURE.
4. APPLY AN APPROVED ALKYD RUST-INHIBITING METAL PRIMER TO ALL EXPOSED IRON METAL SURFACES. MINIMUM 2-COAT APPLICATION OR AS RECOMMENDED BY PAINT MANUFACTURER.

SPECIFIED METAL PRIMER:
SUPER SPEC HP ALKYD METAL PRIMER (P06), BY BENJAMIN MOORE
5. APPLY AN APPROVED EXTERIOR GRADE, ALKYD ENAMEL PAINT TO ALL PRIMED METAL SURFACES. MINIMUM 2-COAT APPLICATION OR AS RECOMMENDED BY PAINT MANUFACTURER. ROLLER-APPLICATION IS NOT PERMITTED.

SPECIFIED METAL FINISH COAT PAINT:
INPERVO EXTERIOR ALKYD HIGH GLOSS ENAMEL
- #2130-10 BLACK BY BENJAMIN MOORE

ORNAMENTAL METAL ELEMENTS & TRIM (CAST-IRON & LEAD-COATED COPPER)

1. TAKE PRECAUTIONS AGAINST LEAD DUST, EYE DAMAGE AND ACCIDENTAL INHALATION OF PAINT DUST PARTICLES. DISPOSE OF LEAD PAINT RESIDUE PROPERLY AS REGULATED BY THE DEPTS. OF BUILDINGS AND SANITATION IN THE CITY OF NEW YORK.
 2. REMOVE ALL LOOSE OR SCALING PAINT BY SCRAPING WITH ABRASIVE HAND TOOLS SUCH AS PAINT SCRAPERS, WIRE BRUSHES, SYNTHETIC STEEL WOOL AND ABRASIVE PADS. LIMITED USE OF ORBITAL AND BELT SANDERS ARE PERMITTED FOR 'FEATHERING' AREAS WHERE PAINT HAS BEEN HAND SCRAPPED. LOW-PRESSURE WASH IS PERMITTED. ON-SITE SANDBLASTING IS NOT PERMITTED.
- LEAD-COATED COPPER (LCC) RESTORATION/ REPAIR
3. RE-ROOF AND FLASH AT EXISTING ORNAMENTAL METAL ROOFS WITH 16 OZ LEAD COPPER. COMPLY WITH THE NATIONAL ROOFING CONTRACTOR'S ASSOCIATION'S CURRENT SPECIFICATIONS AND DETAILS.
 4. PROVIDE 16 OZ. LEAD COATED COPPER PATCH SHEETS AT LOCATIONS OF DETERIORATED EXISTING METAL ELEMENTS. FABRICATE PATCHES TO MATCH HISTORIC MOLDING PROFILE AT EACH LOCATION SHOWN ON THE DRAWINGS. SOLDER ALL SEAMS AND EDGES AT THE PERIMETER OF THE REPAIRED LOCATION. PROVIDE AN IN-FIELD MOCKUP FOR AOR/ LPC REVIEW.

CAST IRON RESTORATION / REPLACEMENT

5. PATCHING/ SPLICE REPAIRS: APPLY AN EPOXY RESIN FILLER COMPOUND CONTAINING IRON PARTICLES APPROVED FOR HISTORIC RESTORATION OF EXISTING CAST-IRON. APPLY FILLER COMPOUND TO SUPERFICIAL, NON-STRUCTURAL CRACKS AND SURFACE DEFECTS OF THE CAST IRON. SPLICING OF REPLACEMENT CAST IRON SECTIONS SHALL BE REVIEWED ON AN INDIVIDUAL BASIS BY THE ARCHITECT. APPROVED SPLICE REPAIRS SHALL BE MECHANICALLY FASTENED WITH STAINLESS STEEL BOLTS/ SCREWS.
6. REPLACEMENT: IN THE EVENT THAT LARGE PORTIONS OF EXISTING CAST IRON ELEMENTS ARE SEVERELY CORRODED, DAMAGED OR PARTLY ENTIRELY MISSING, THE CAST IRON PIECE SHALL BE THOROUGHLY DOCUMENTED BY VISUAL SURVEY, FIELD MEASUREMENTS, SCALED SHOP DRAWINGS, RUBBINGS AND CASTINGS OF RECONSTRUCTED PORTIONS (OF WOOD OR PLASTIC) OF EACH PIECE AND/OR AN IDENTICAL/ SIMILAR EXISTING DECORATIVE CAST IRON ELEMENT OCCURRING ELSEWHERE ON THE BUILDING OR ADJACENT 'SISTER' BUILDING. REPLICA CASTINGS SHALL BE FABRICATED AT AN APPROVED METAL-IRONWORKS FOUNDRY, WITH EXPERIENCE IN HISTORIC DECORATIVE CAST IRON DETAILING. ALL SURFACES OF FINISHED REPLICA CASTINGS SHALL BE SHOP PRIMED PRIOR TO DELIVERY ON-SITE TO PREVENT RUSTING. ALL PIECES SHALL BE ASSEMBLED AND SECURED IN-FIELD WITH STAINLESS STEEL FASETENERS.
7. ALL EXISTING RESTORED AND REPLACED CAST IRON SHALL BE CAULKED WITH ARCHITECTURAL GRADE POLYURETHANE SEALANT AT ALL JOINTS/ SEAMS.

PAINTING METAL (CAST-IRON/ LCC)

8. WIPE DOWN LEAD COATED COPPER SURFACES WITH MINERAL SPIRITS TO REMOVE ANY REMAINING MOISTURE.
9. APPLY AN APPROVED ALKYD RUST-INHIBITING METAL PRIMER TO ALL EXPOSED ORNAMENTAL LEAD COATED COPPER SURFACES. MINIMUM 2-COAT APPLICATION OR AS RECOMMENDED BY PAINT MANUFACTURER.

SPECIFIED METAL PRIMER:
SUPER SPEC HP ALKYD METAL PRIMER (P06), BY BENJAMIN MOORE
10. APPLY AN APPROVED EXTERIOR GRADE, ALKYD ENAMEL PAINT TO ALL PRIMED LEAD COATED COPPER SURFACES. MINIMUM 2-COAT APPLICATION OR AS RECOMMENDED BY PAINT MANUFACTURER. ROLLER-APPLICATION IS NOT PERMITTED.

STREET FAÇADE CORNICE
- #2130-10 BLACK BY BENJAMIN MOORE

ORNAMENTAL IRON WORK & STREET ROOF CORNICE:
2132-10 BLACK, BY BENJAMIN MOORE

WOOD FRAME WINDOWS THROUGHOUT & EXISTING ENTRY DOOR
HC-70 VAN BUREN BROWN, BY BENJAMIN MOORE

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SE CORNER VIEW OF 21.5 KING STREET FROM CORNER OF KING ST. & 6TH AVE.



SE CORNER VIEW OF 21.5 KING STREET FROM CORNER OF KING ST. & 6TH AVE.



SOUTH FACADE 2ND THROUGH 5TH FLRS - 21.5 KING STREET



FRONTAL VIEW OF BUILDINGS ALONG KING STREET FROM OPPOSITE SIDEWALK



SE CORNER DETAIL VIEW OF 21.5 KING STREET FROM OPPOSITE STREET CORNER



SOUTH FACADE AT 1ST FLOOR - 21.5 KING STREET

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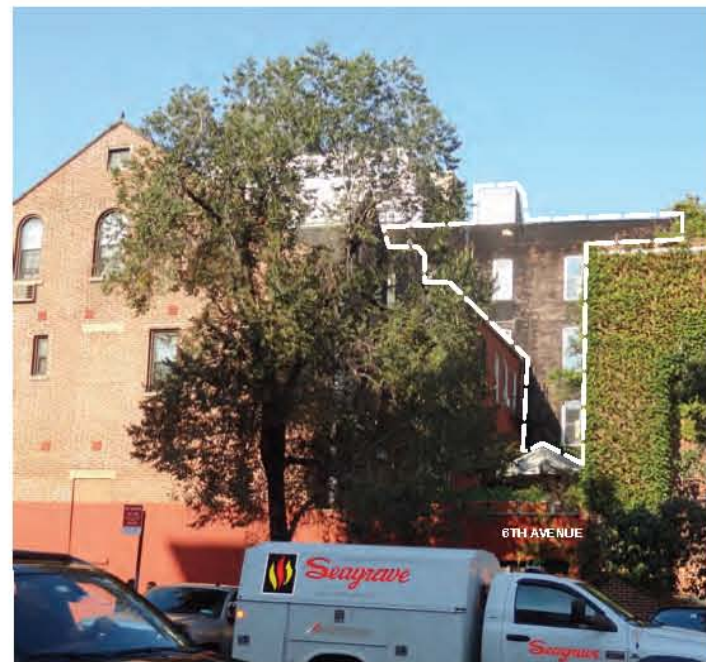
6TH AVENUE DETAIL VIEW OF EAST WALL SEEN THROUGH REAR YARD / INTERIOR OF BLOCK



6TH AVENUE VIEW OF BLOCK SEEN FROM OPPOSITE SIDEWALK



WEST HOUSTON ST. VIEW OF REAR YARD (NORTH) FACADE THROUGH INTERIOR BLOCK



SOUTH WESTERN VIEW ALONG 6TH AVE SEEN FROM OPPOSITE SIDEWALK



SOUTH WESTERN VIEW ALONG 6TH AVE SEEN FROM OPPOSITE SIDEWALK

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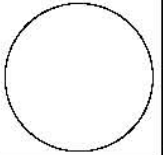
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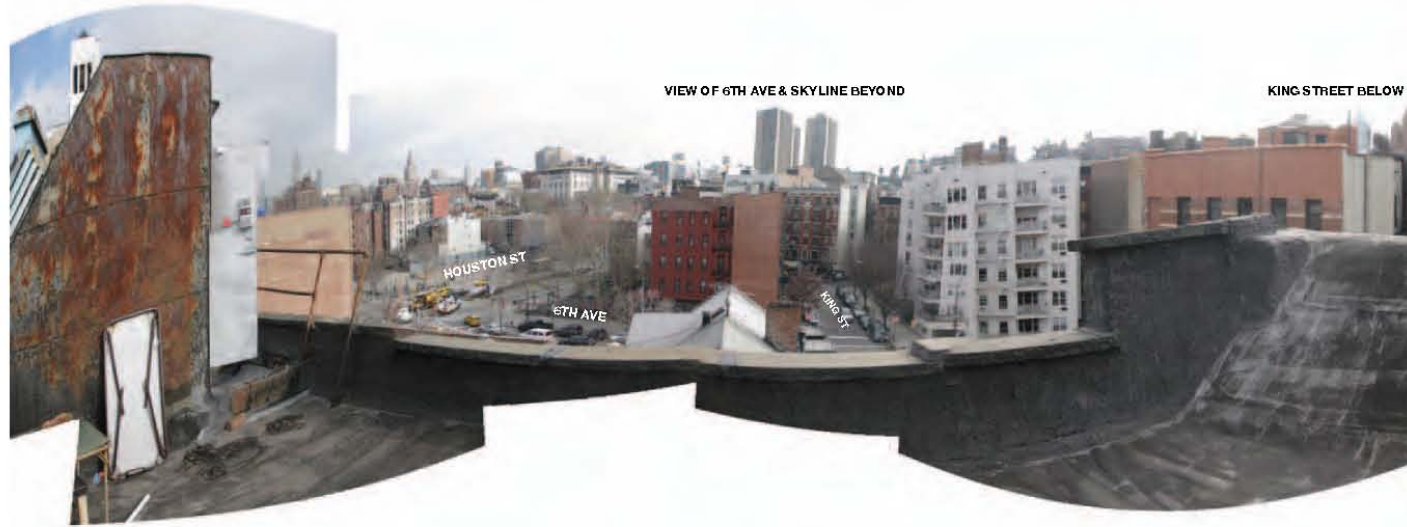


VIEW OF EXISTING CENTER WEST LIGHTWELL FROM 2ND FLOOR TO 5TH FLOOR



VIEW OF EXISTING NORTH WEST LIGHTWELL FROM 1ST FLR TO ROOF

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VIEW OF 6TH AVE & SKYLINE BEYOND

KING STREET BELOW

EAST PANORAMA VIEW FROM EXISTING ROOF



EASTWARD AERIAL VIEW OF ADJACENT REAR YARDS BELOW



ADJACENT ROOF OF 23 KING STREET

WEST PANORAMA VIEW FROM EXISTING ROOF



EXISTING ROOF (LOOKING SOUTH)



EXISTING SOUTHEASTERN PARAPET CORNER (6TH AVE BEYOND)



EXISTING CENTER LIGHTWELL ALONG WEST LOT LINE (LOOKING NORTH)

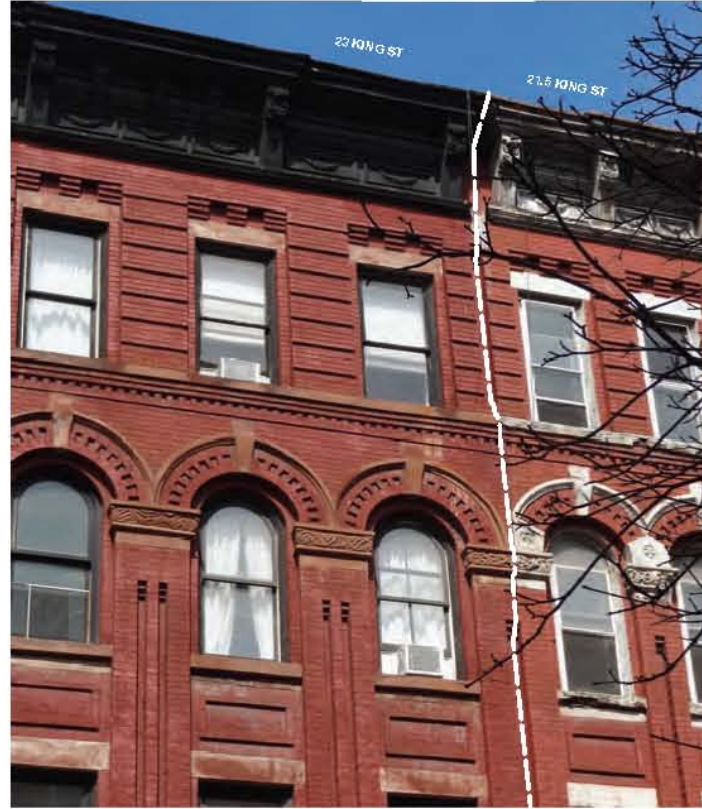


EXISTING BULKHEAD & LIGHT WELL ALONG WEST LOT LINE (LOOKING SOUTH)

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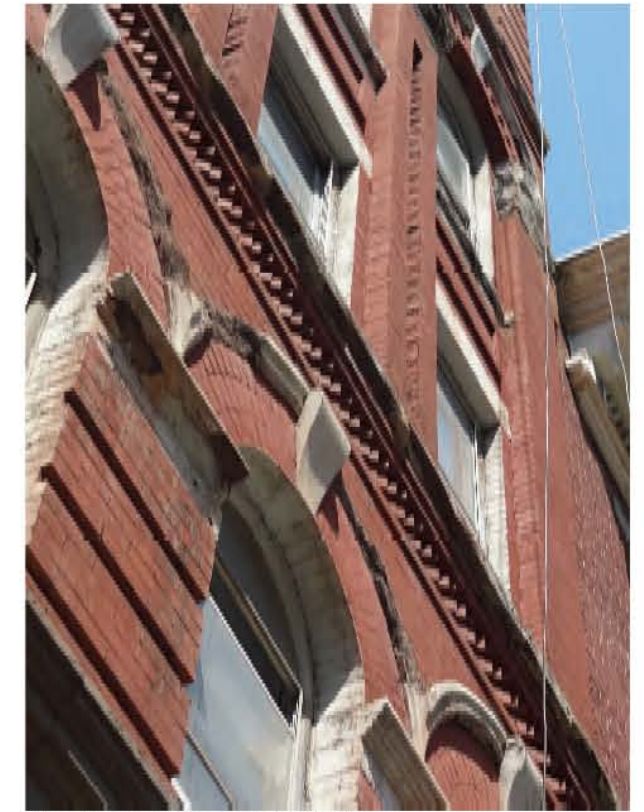
CORNICE, 4TH & 5TH FLOOR WINDOWS AT SOUTH FACADE
 - PLEXI-VINYL STORM WINDOW APPLIED OVER EXISTING WOOD WINDOW
 - HEAVILY SPALLED & PAINTED BROWNSTONE MOULDINGS & DETAILS
 - RUST DAMAGE AT PORTIONS OF CAST IRON CORNICE



23 KING STREET (FACADE RESTORED IN 2005) & 21.5 KING STREET (EXISTING CONDITION)



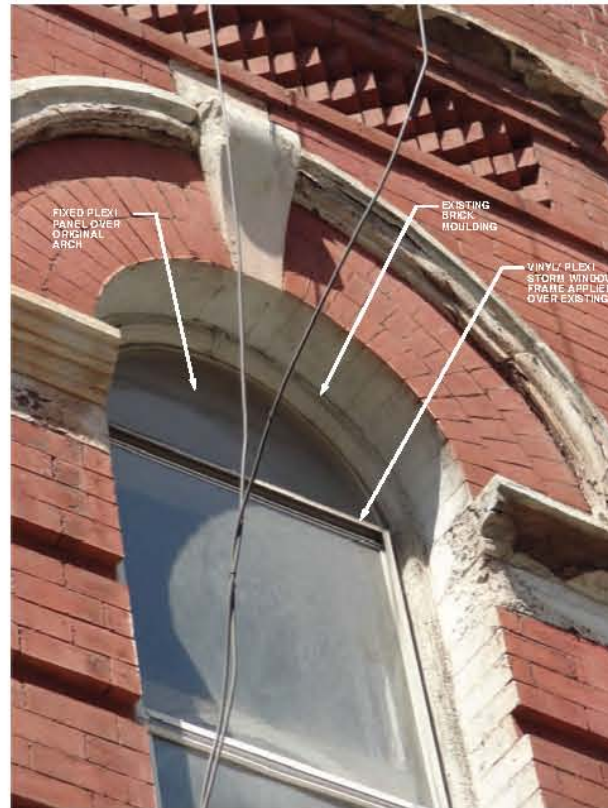
INTERIOR & EXTERIOR DETAIL VIEWS OF TYPICAL PLEXI-VINYL STORM WINDOW AT SOUTH FACADE. PLEXI PANEL OVER EXISTING DOUBLE HUNG WINDOWS W/ ARCHED GLASS LIGHT IN UPPER SASH.



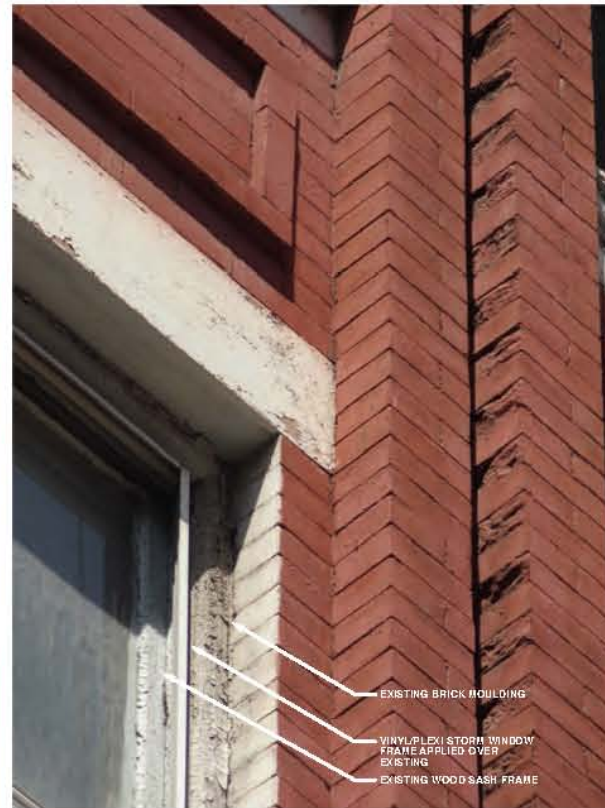
2ND, 3RD & 4TH FLOOR WINDOWS AT SOUTH FACADE
 - HEAVILY SPALLED & PAINTED BROWNSTONE MOULDINGS & DETAILS
 - WEATHER DAMAGED BRICK



3RD & 4TH FLOOR WINDOWS AT SOUTH FACADE
 - PLEXI-VINYL STORM WINDOW APPLIED OVER EXISTING WOOD WINDOW
 - HEAVILY SPALLED & PAINTED BROWNSTONE MOULDINGS & DETAILS



DETAIL OF 2ND FLOOR WINDOW AT SOUTH FACADE
 - PLEXI-VINYL STORM WINDOW APPLIED OVER EXISTING WOOD WINDOW



DETAIL OF 3RD FLOOR WINDOW AT SOUTH FACADE
 - PLEXI-VINYL STORM WINDOW APPLIED OVER EXISTING WOOD WINDOW



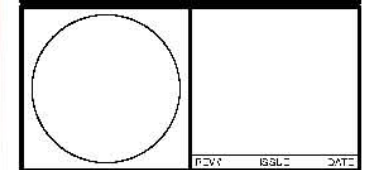
DETAIL OF 4TH FLOOR WINDOW AT SOUTH FACADE
 - PLEXI-VINYL STORM WINDOW APPLIED OVER EXISTING WOOD WINDOWS
 - HEAVILY SPALLED & PAINTED BROWNSTONE MOULDINGS & DETAILS

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