

+selectbuservice

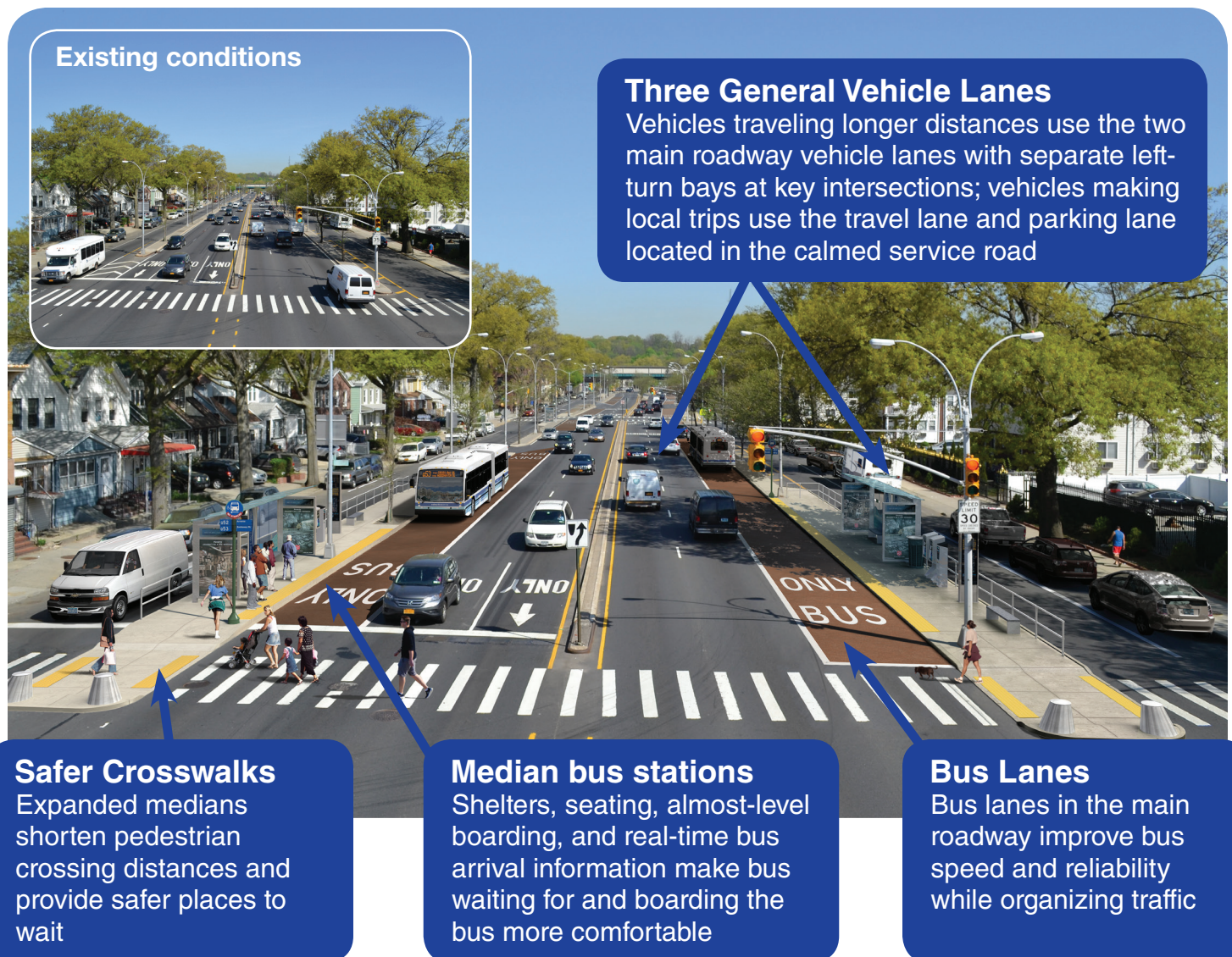
Woodhaven and Cross Bay Boulevards

Newsletter 2 / Spring 2016

Proposed designs for Q52/53 Select Bus Service

NYC DOT and the MTA have worked together to incorporate over two years of community feedback to present a redesigned plan for the Woodhaven / Cross Bay Boulevard Q52/Q53 Select Bus Service project that will bring high-quality transit service to Queens and a safer and more efficient street for all users. A new phased implementation plan that brings the benefits of SBS sooner with updated street and intersection designs followed by a longer-term capital project, will greatly improve the pedestrian, bus riding, and driving experience along the corridor.

Example rendering of SBS stations at Woodhaven Boulevard & 91st Avenue



Phased project implementation timeline

2017 short-term project

To bring improvements to the corridor quickly, the Q52/Q53 SBS project will be launched in phases. The first phase will include several roadway and transit improvements that will be constructed in 2017:



Off-board fare collection: SBS customers pay their fare at machines located at SBS stops before boarding, reducing the time to pick up and drop off passengers.



Pedestrian safety enhancements: New medians, pedestrian waiting areas, and crosswalks improve the safety and comfort of pedestrians and bus riders along the corridor.



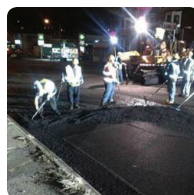
Bus lanes: Bus lanes along Woodhaven and Cross Bay Boulevards bring faster and more reliable service to more than 30,000 daily bus riders.



Median bus stations: Bus stops along 1.3 miles of Woodhaven Blvd where service roads now exist will be a better place to wait for the bus with added amenities such as real time passenger information, public art, and seating.



Transit Signal Priority (TSP): Q52/Q53 SBS buses will have TSP technology, which improves bus and general travel times by holding a green light longer or shortening a red light faster when a bus is approaching.



Street resurfacing & markings: Portions of Woodhaven and Cross Bay Blvd will be resurfaced to provide a smoother ride for bus riders and drivers alike.

Longer-term capital project

In partnership with the NYC Department of Design and Construction (DDC), the second phase will be a longer-term capital project to build more complicated improvements along Woodhaven / Cross Bay Blvd including **additional median bus stations** along Woodhaven Blvd as well as:

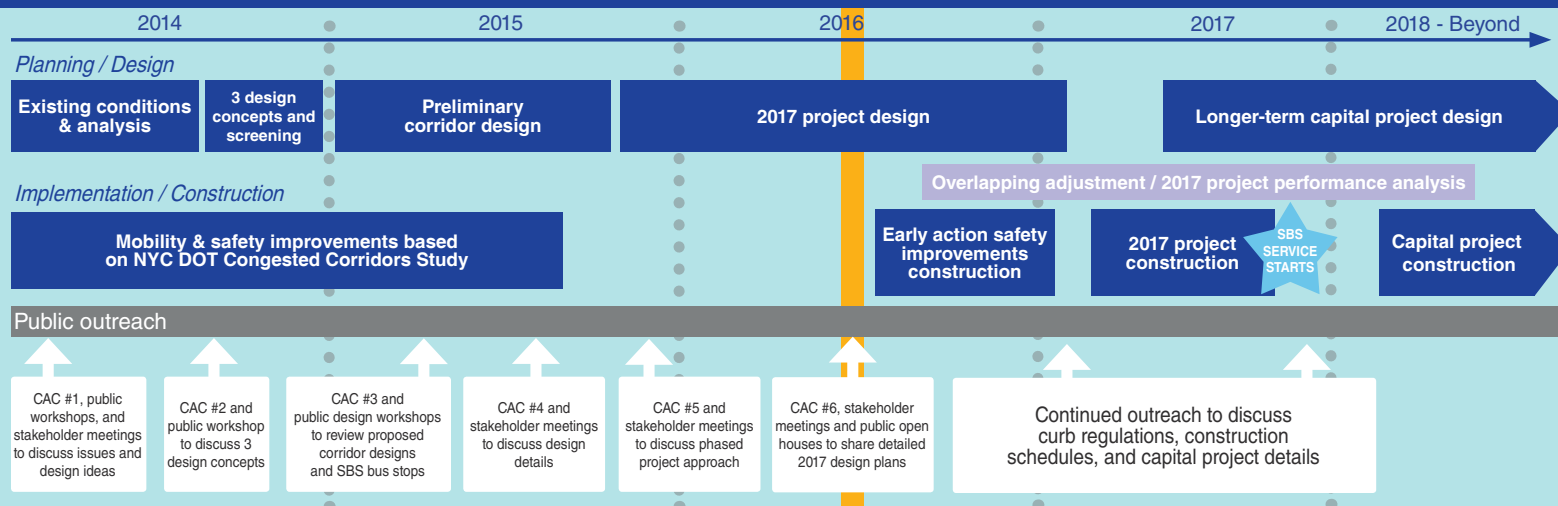


Medians and plantings: expanded medians and pedestrian improvements provide for extra pedestrian comfort and safety while allowing opportunities for plantings along the corridor.



Bus Bulbs on Cross Bay Blvd: Expanding the sidewalk to meet the bus at a bus stop provides more room for bus riders to wait and allow buses to travel in and out of the stop without pulling over.

Project Timeline



2017 project highlights



Bus lanes

- Existing offset bus lanes on Woodhaven Blvd between Eliot Ave and Metropolitan Ave will remain
- New bus lanes between on Woodhaven/Cross Bay Blvd between Union Tpke and Belt Pkwy (see map on back page)
- No bus lanes on Cross Bay Blvd south of the Belt Pkwy in Howard Beach



Bus stations

- Median stations on Woodhaven Blvd where service roads already exist (Park Lane South to Rockaway Blvd)
- Exploring unique, taller fencing with specialized design to improve safety for bus riders standing on median stations
- Improved curbside stations elsewhere



Safety

- Curb extensions and refuge islands at key pedestrian crossings along corridor
- New mid-block pedestrian crossings
- Reconfigured medians on Woodhaven Blvd between Union Tpke and the Jackie Robinson Pkwy overpass to address vehicle speed/collisions



General traffic

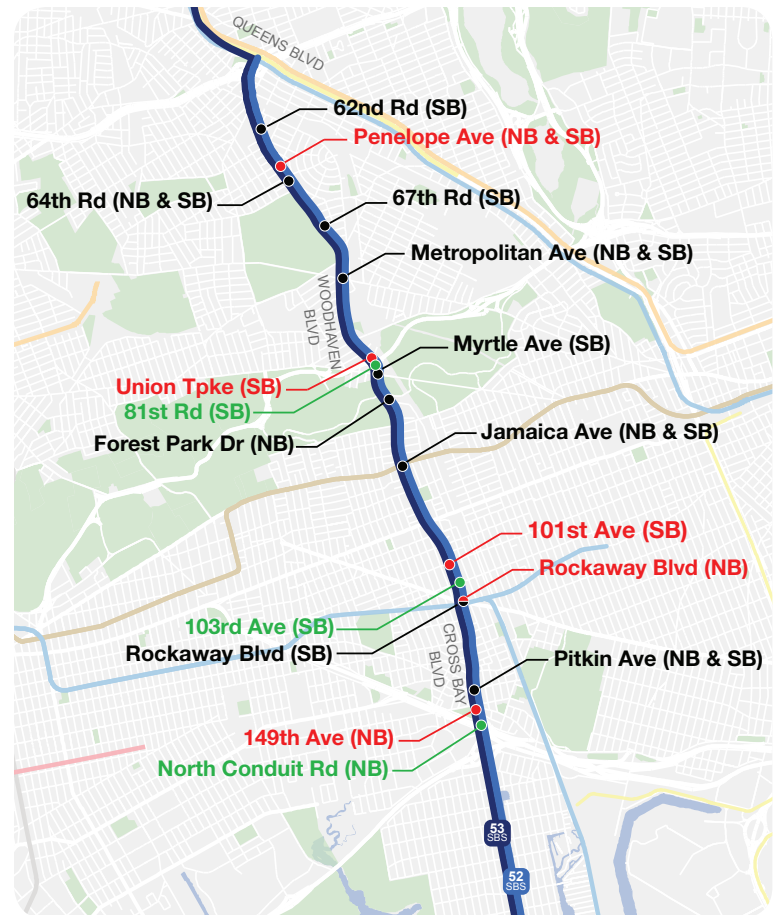
- Three lanes of general traffic will remain in each direction
- New slip locations adjusted based on community comments



Left turns

- Left turns preserved / added for local access
- Key left turns restricted to improve traffic flow and to reduce vehicle and pedestrian conflicts

Map of key left-turns along Corridor

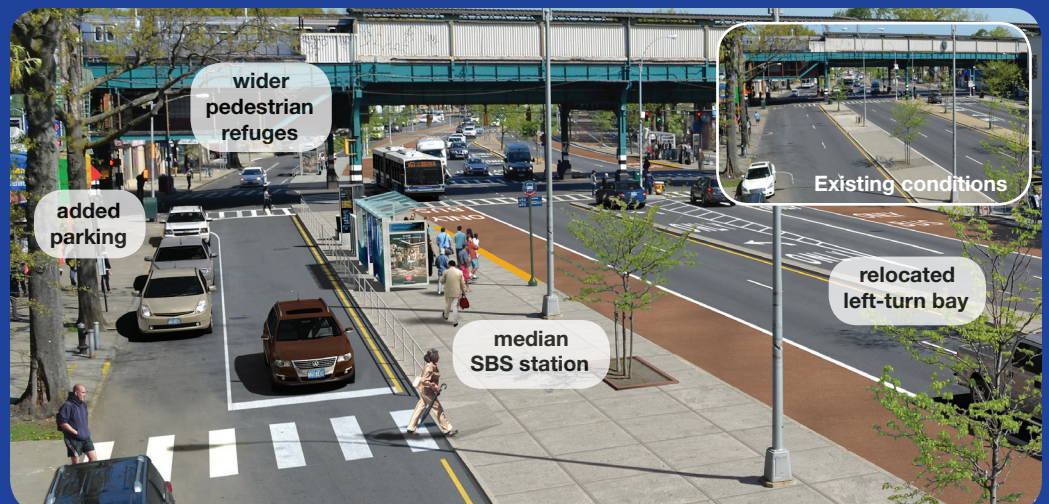


Legend

- Left turn added
- Left turn preserved
- Left turn restricted

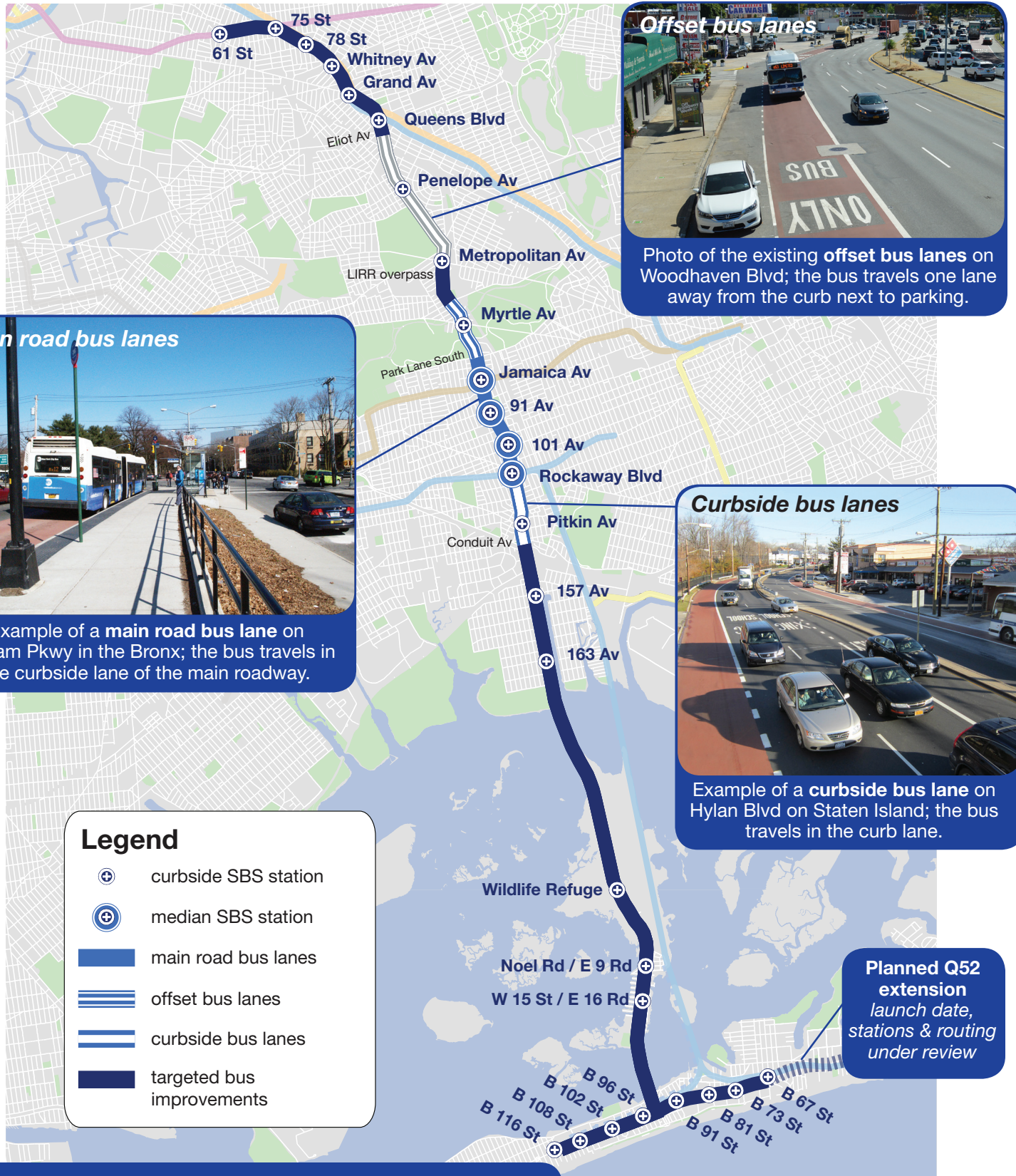
Design spotlight: Jamaica Avenue

Woodhaven Blvd at Jamaica Ave is a Vision Zero Priority Intersection and a major transportation hub for Woodhaven, Queens. With continued community input, DOT redesigned the plans for the intersection to include enhanced pedestrian crossings to improve pedestrian safety and relocated left-turn bays to preserve access to Jamaica Ave while addressing vehicle safety issues.



Example rendering of Woodhaven Boulevard at Jamaica Avenue (looking north)

Q52/Q53 Select Bus Service Corridor



Stay Connected!

Visit our website www.nyc.gov/brt for updated project information or contact **Matt Kroneberger**, Community Outreach Coordinator, at **212-839-7289** or email us at brt@dot.nyc.gov



@NYC_DOT



facebook.com/
NYCDOT



@NYC_DOT